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Setting the scene

1.0 SETTING THE SCENE

1.1 INTRODUCTION

1.1.1 PURPOSE OF THIS DOCUMENT

The North Blackburn Development Site is a housing allocation site in the Council’s Local Plan Part 2: Site Allocations and Development Management Policies, adopted December 2015 .

The Local Plan identifies this site as a strategic housing site in northern Blackburn and promotes its development as a key component in the delivery of the Council’s strategy for housing and economic growth. The site presents a great opportunity to provide a wider choice of housing, principally family homes and in an attractive setting on the edge of the countryside.

The site covers approximately 24 hectares and will potentially deliver up to 450 new homes.

There are a number of land owning parties who are identified in Figure 1.

This masterplan document has been prepared in collaboration with the landowners in response to Policy 16/2 – North Blackburn Development Site, Blackburn with Darwen Local Plan Part 2: Site Allocations and Development Management Polices. A key consideration of the housing land allocation policy is that:

- *‘The site is to be brought forward in line with a masterplan to be produced covering the whole of the allocation. The masterplan must be agreed by the Council prior to the granting of planning permission for any part of the site.’*

The masterplan will be a material consideration in the review of and decision making on every planning application for development on this site.

The Local Plan policy requirement is for a site wide framework anticipated that delivery would most likely be phased over time through the plan period, in response principally to local housing market conditions and the multiple land ownerships of individual land parcels across the site.

With this in mind the purpose of this masterplan framework is to achieve the comprehensive masterplanning of development across the whole site, the coordination of development, the timely delivery of supporting infrastructure and to ensure high standards of design in scheme layout and detailing.

1.1.2 AIMS OF DOCUMENT

The overarching aims of the framework are to:

- Set out a comprehensive development framework for the masterplan area identifying the site wide over-arching structural elements that need to be taken into account by each and every development scheme on this site to ensure that the form and content of the overall development comes forward in a complementary and coherent manner;
- Identify design principles for the development that will ensure high standards of design in scheme layout and detailing;
- Set out the planning policy requirements and evidence base for the preparation of scheme proposals on the site;
- Guide and inform the determination of individual planning applications including advice on developer contributions; and
- Inform future management and ongoing maintenance.

This document sets out the technical work carried out by the landholders and their consultants, with specific reference to the baseline evidence which has been prepared to date. This evidence has been summarised in the form of a constraints plan (section 1.7 page 37) and a concept masterplan framework (section 2 page 45) for the site.

1.1.3 SITE DESCRIPTION

The site is importantly located on the northern edge of Blackburn overlooking the countryside; it is adjacent to the Green Belt and Ribble Valley. It is also close to Blackburn Town Centre, approximately 4km away; directly adjacent to the A6119 outer ring road and has excellent accessibility to the wider strategic highway network, including junction 31 of the M6 and junction 6 of the M65.

Ramsgreave and Wilpshire railway station is 2.1km from the site providing services to Blackburn, Manchester and Preston (west coast main line). Regular bus services operate through the district centres at Brownhill and Whalley New Road.

The site is also well located to local facilities. There are local centres nearby to the east of the site along Whalley New Road and at Brownhill. At Brownhill there is a Co-op foodstore, GP surgery, fish and chip shop, florist, bakery, laundrette and pharmacy. There is also a small number of shops directly to the south of the site along Pleckgate Road.

Pleckgate High School is directly to the south of the site and there are a

number of primary schools in the local area, including Lammack Primary School, St. Gabriels Primary School and Holy Souls Roman Catholic Primary School all of which are located on the south side of Ramsgreave Drive.

The site is well connected by rights of way to open countryside and Mellor Ridge to the north. To the south, the site has the potential to connect into a strategic green corridor extending from Pleckgate High School and playing fields, through Blackburn Golf Course to Corporation Park on the edge of the town centre.

Part of the site includes Blackburn Rugby Club which comprises four full sized rugby pitches, a further 3/4 sized pitch, club house and associated facilities, landscaping, car parking and hard standing.

Part of the site within the ownership of the Rugby Club is within Ribble Valley Borough Council (RVBC). However, its potential use has been considered as part of the development and going forward, will be discussed with RVBC.

KEY

- Pauline Taylor + Gordon Driver
- Jean Matthews
- BRUFC
- James + Peter Butterfield
- Sarah + Roger McLennan

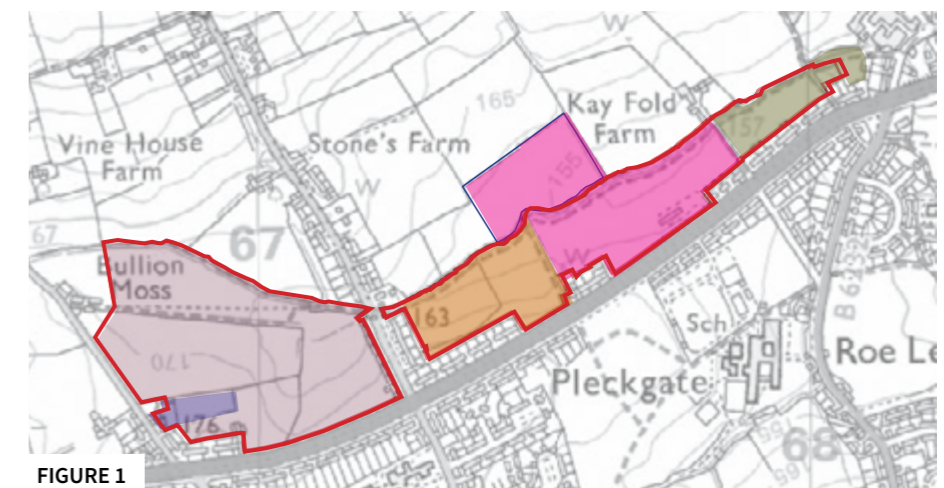


FIGURE 1

PLAN ILLUSTRATING LANDOWNERS WITHIN THE MASTERPLAN AREA

MASTERPLAN SITE BOUNDARY



FIGURE 2
DEVELOPMENT SITE LOCATION PLAN

1.0 SETTING THE SCENE

1.2 PLANNING CONTEXT

The following section sets out the planning policy context for developing the masterplan area. It provides a summary of national policy requirements and guidance, as well as identifying the key elements of the Council’s development plan which would need to be taken into account in the detailing of individual scheme proposals and to inform decision making on all planning applications.

1.2.1 NATIONAL PLANNING POLICY FRAMEWORK (NPPF)

The National Planning Policy Framework (NPPF) sets out the Government’s planning policies and how these are expected to be applied to development.

The NPPF promotes sustainable development. With regards to housing NPPF encourages development that delivers a wide choice of high quality homes, widens opportunities for home ownership and contributes to the creation of sustainable and inclusive communities. Para 50 notes that to achieve this Councils are advised to:

- ‘Plan for a mix of housing based on current and future demographic trends, market trends and the needs of different groups in the community.....including families with children and older people....’;

The NPPF also places considerable importance on securing high quality and inclusive design. It recognises that good design is a key aspect of sustainable development and should contribute positively to making places better for people. One of its core planning principles is planning should:

- ‘Always seek to secure high quality and inclusive design and a good standard of amenity for all existing and future occupants of land and buildings;’

National Planning Practice Guidance (PPG) provides further guidance on securing high standards of design in new development. It emphasises the importance of design in successfully integrating development with its surrounding context. Additionally, the guidance encourages new development to be distinctive, responsive to natural features and locally characteristic patterns of development, provide attractive and well-connected permeable street networks and integrate a system of easily accessible open and green spaces.

The NPPF needs to be read alongside other national planning policies relevant to the development on individual sites. In this case this would include consideration of the Government’s expectations for sustainable

* House of Commons Written Statement (HCWS161) Sustainable drainage systems

drainage systems to be provided as an integral part of new development*. National policy is a material consideration in the review of and decisions making on all planning applications.

1.2.2 LOCAL DEVELOPMENT PLAN

The Council’s local development plan consists of the Core Strategy (Local Plan Part 1) and the Local Plan Part 2: Site Allocations and Development Management Policies.

Core Strategy (Local Plan Part 1) (adopted January 2011)

This sets the strategic framework for planning and development in the borough to 2026 to meet the Council’s growth agenda. It includes a suite of thematic, spatial policies including land supply for business development/ quantity, quality and mix of housing/provision of public facilities/ protecting and enhancing the environment/place making and access to jobs and services. A copy of the document can be viewed on the Council’s website:

www.blackburn.gov.uk/Lists/DownloadableDocuments/Adopted_Core_Strategy_final_A4.pdf

The masterplan seeks to deliver a number of the Core Strategy’s key objectives including: -

- To promote housing growth. The Council needs housing growth to rebalance its housing market and to underpin economic growth;
- To improve the housing offer in the borough by providing a wider choice of homes (type/size/tenure and quality), in particular larger family dwellings;
- To retain and attract skilled and qualified people to live in the borough;
- To protect and enhance the quality of the local environment;
- To deliver quality placemaking;
- To promote sustainable development.

The Council’s affordable housing policy set out in Policy CS8: Affordable Housing Requirements will apply.

Local Plan Part 2: Site Allocations and Development Management Policies (adopted December 2015)

The Local Plan Part 2 supplements the development strategy set out within the Core Strategy and supports its implementation by identifying strategic land allocations for a range of uses including housing, employment and green infrastructure and a suite of development management policies that will apply to every development proposal that requires planning permission.

The Part 2 plan includes a set of 12 core policies that clarify the Council’s approach to a range of key issues, such as impact and appearance that apply to all development. Additional policies are theme based in line with the spatial interventions set out in the Core Strategy.

The plan’s housing land allocations are fundamental to the delivery of the Council’s overall strategy for housing growth and include a range of key sites within the inner urban area, on the edge of the urban area and in the rural areas of the borough.

The masterplan area is a key part of a housing land allocation located on the edge of the urban area; Policy 16/2 – North Blackburn Development Site. The site is an urban extension onto land previously designated as Green Belt along the borough boundary with Ribbles Valley. The Local Plan Part 2 has released the site from the Green Belt.

Policy 16/2 identifies a number of site specific development considerations that will need to be taken into account alongside other local plan policies as part of the development process including scheme detailing and obtaining planning consent. A primary consideration relates to the status of the masterplan; the policy requires that:

- ‘The site is to be brought forward in line with a masterplan to be produced covering the whole of the allocation. The masterplan must be agreed by the Council prior to the granting of planning permission for any part of the site.’

A copy of Policy 16/2 which sets out the full list of site specific considerations is provided in Appendix C (page 93) for ease of reference.

With the exception of those items requiring a more detailed survey/ assessment the key considerations are addressed in the masterplan.

1.0 SETTING THE SCENE

1.2 PLANNING CONTEXT

Policy 16/2 notes that the development will need to:

‘Make land available for a new primary school, if required, and suitable contribution towards its construction.’

During the time of the preparation of the masterplan an assessment of the education provision in the borough was undertaken by the Council’s Children’s Services Department. This concluded that there is sufficient capacity within local primary schools to meet the pupil yield from the anticipated housing growth for the initial few years of the site’s growth. This will be kept under review and proposals to create additional primary school places for the area are being developed with the intention being to expand existing school provision to meet the additional demand for places.

There are a small number of housing parcels within the land allocation boundary that, at the owners’ request, are not included within the masterplan area. If/when a planning application/s comes forward for development on these sites the proposals will be expected to complement and comply with the masterplan requirements.

A copy of Local Plan Part 2 can be viewed on the Council’s website:

www.blackburn.gov.uk/newlocalplan

1.2.3 SUPPORTING PLANNING GUIDANCE

There are a number of other documents which should be referred to when developing proposals for the site. These include:

- **Borough Design Guide**
This document outlines a series of principles for securing high quality urban design and provides general guidance to promote better design on individual sites/developments. It seeks to raise awareness of good design and encourages an innovative design-led approach to development.
- **Residential Design Guide**
This document provides targeted advice on how the Council expects

developers to enhance the quality of new homes and residential places across the borough. It seeks to ensure that new development reflect the area’s special character and promotes the highest standards in design¹.

- **Green Infrastructure & Ecological Networks Supplementary Planning Document (SPD)**
The SPD provides guidance to help applicants and developers to ensure that proposals for development make the most of opportunities to improve existing and create new green infrastructure and ecological networks.
- **Travel Plan Supplementary Planning Document (SPD)**
The document outlines the Council’s requirements for the development, implementation and management of Travel Plans. The SPD supports developers and consultants during the planning application process. It is also intended for use by businesses/organisations who wish to develop a Travel Plan to facilitate more efficient and sustainable working practices².

These documents can be accessed on the Council’s website:
www.blackburn.gov.uk/Pages/Supplementary-Planning-Documents-.aspx

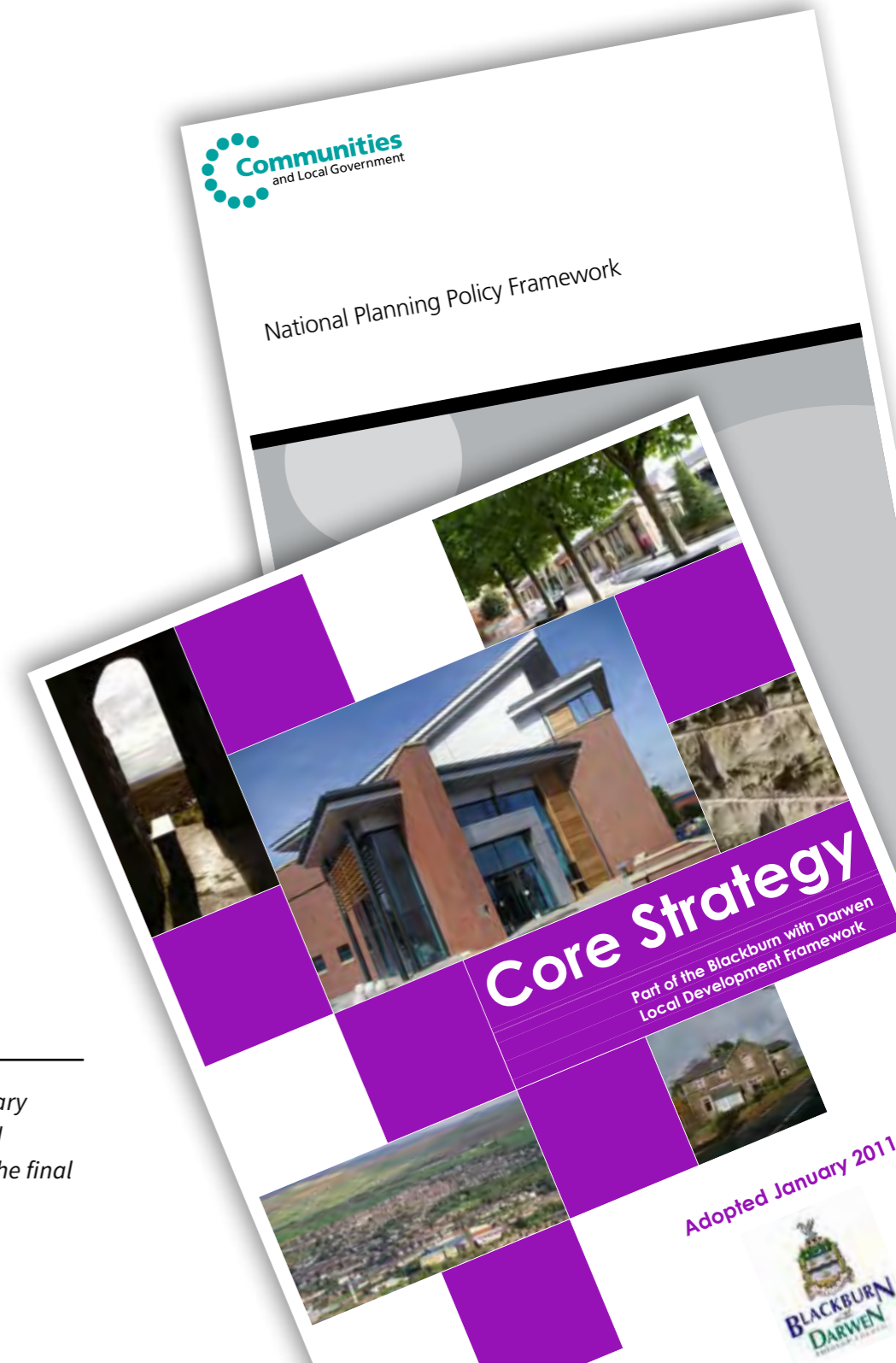
- **Parking standards**
The Council’s parking standards can be found at:
www.blackburn.gov.uk/Lists/DownloadableDocuments/Parking%20Standards%20Adopted%20April%202014.pdf

The above should not be regarded as a comprehensive list and can be discussed further as part of the planning application stages of work.

The masterplan will, alongside Local Plan policies be a material consideration in the review of and decision making on all planning applications for development on this site. It will also help to secure suitable developer contributions for identified infrastructure requirements. Further detail on phasing and delivery, developer contributions and planning application requirements is set out in Section 3, page 60.

1. The Residential Design Guide (revised edition September 2012) is currently being updated. The updated version is due to be adopted as an SPD during 2017.

2. Local Plan Part 2 proposes the preparation of a Travel Plan Supplementary Planning Document (SPD) to support delivery of Policy 10: Accessibility and Transport. A draft version of the SPD has been prepared for consultation. The final version is due for adoption during 2017.



1.0 SETTING THE SCENE

1.3 DESIGN GUIDANCE

Securing Design Quality

Development will be required to apply high standards of design as well as being well designed and delivered in accordance with best practice. This will ensure that the overall vision for North Blackburn Development Site is for a highly desirable place to live with a distinct local identity.

Scheme proposals will need to demonstrate how they have responded to the setting and characteristics of the site, scheme requirements and design principles that are set out in Section 2 of this document.

In addition to the guidance contained within this masterplan, a range of supplementary measures are encouraged in order to achieve the required design quality objectives for development including:

- Building for Life
- Design Review
- Secured by Design

Building for Life

Building for Life is a government-endorsed industry standard for well-designed homes and neighbourhoods.

Building for Life 12 is a tool for assessing the design quality of homes and neighbourhoods that was developed by the Commission for Architecture and the Built Environment (CABE) (now under the umbrella of the Design Council) in partnership with the Home Builders Federation. It provides a tangible set of measures for assessing how successfully a new neighbourhood has been designed.

Development proposals should seek to respond to the Building for Life criteria and applicants will be encouraged to submit completed Building for Life assessments alongside any full or reserved matters application.

Design Review

Places Matter is an organisation devoted to generating a strong sense of place in living, working and leisure environments throughout the North West.

Places Matter provides a design review service which aims to improve the quality of new development by offering constructive and impartial advice.

Where appropriate, applicants will be encouraged to submit their schemes for independent design review prior to the proposals being finalised and the planning application submitted.

Further information is available:

www.placesmatter.co.uk/design-review/what-is-design-review/

Secured by Design

Secured by Design (SBD) is a police led initiative providing guidance on crime prevention measures in the detailed design and delivery of new homes. The advice is based on established principles for designing out crime which have been proven to reduce the opportunity for crime and the fear of crime, creating safer, more secure and sustainable neighbourhoods.

Secured by Design is owned by the Association of Chief Police Officers (ACPO) and is supported by the Home Office and the Department for Communities and Local Government (DCLG).

Core policies in the Local Plan Part 2, in particular Policy 8: Development and people and Policy 11: Design set out the Council's requirements for creating safe and attractive neighbourhoods.

Crime prevention is a material consideration in the determination of planning applications.

Further information is available in the 2016 edition of SBD New Homes:

www.securedbydesign.com/wp-content/uploads/2014/02/SBD-NewHomes14-lowres.pdf

1.0 SETTING THE SCENE

1.4 CONSULTATION

A key consideration for the preparation of the masterplan was to ensure that the form and functioning of the development takes into account, as much as possible, local issues and the concerns of established local residents. It is important that this new neighbourhood on the edge of the urban area becomes integrated within the surrounding area and is well connected to established communities, surrounding local facilities and services.

With this in mind public consultation took place at an early stage in the preparation of the masterplan enabling resident feedback to sit alongside other baseline evidence in the shaping of the development framework and supporting development principles set out in this document.

Public consultation on initial design concepts for the masterplan area was held over a six week period from Monday 14 March – Monday 23 April including two public drop-in events held in local schools. Over 100 residents including people living in Blackburn and in the Ribble Valley district attended the events; 81 responses were received including a petition with over 300 signatures objecting to a new road junction/access off Whinney Lane. The responses included a wide range of comment/ observations on the consultation material including objections to the development as a whole or particular aspects; support for the development alongside providing local information.

The main concerns raised in the feedback are summarised below under the following theme headings: -

1. Transport – highways/access and connectivity
2. Loss of open space/impact on wildlife and biodiversity
3. Design
4. Flood risk and surface water drainage
5. Impact on amenity of existing residents
6. Impact on local infrastructure

1. Transport - Highways/ access and connectivity:

- Need for a Transport Assessment to assess the impact of the development on the existing highway including M65 junction 6, the local network, in particular the Brownhill roundabout. The Transport assessment should identify required mitigation works;
- Impact of road junction/access onto Whinney Lane/creation of a ‘rat-run’ through the development and impact of increased volume of traffic on highway safety for both cars and pedestrians;
- Concerns regarding number of junctions onto Yew Tree/Ramsgreave Drive;
- Need for development to encourage and provide for sustainable methods of transport including pedestrian and cycle connectivity within the site and to wider urban area including potential to link to the planned Weaver’s Wheel cycle route;
- Need to retain access to properties at Kay Fold Farm;
- Enhancement of existing footpaths to provide connectivity with surrounding facilities including local neighbourhood centres/schools and green spaces.

2. Loss of open space/impact on wildlife and biodiversity:

- Green infrastructure framework regarded as particularly important;
- Green corridor to be multi-functional;
- Loss of open space and impact on wildlife and ecological networks. Consideration needs to be given to importance of links to biodiversity. Sensitive habitats need to be protected. Trees/hedgerows and watercourses need to be enhanced to support habitats.

3. Design:

- Concerns regarding the scale of development;
- Views through the site to the wider countryside should be retained and reflected in design principles;
- Existing features such as dry stone walls should be retained.

4. Flood risk and surface water drainage:

- Concerns over surface water flooding;
- Development should integrate sustainable drainage systems;
- Incorporate requirements for water efficiency measures in scheme proposals.

5. Impact on amenity of existing residents:

- Impact of development on amenity of existing residents/health and well-being needs to be taken into account – during construction and as a result of development;
- Impact of development on residents in Ribble Valley, especially in Mellor, needs to be considered;

6. Impact on local infrastructure:

- Impact on local services and facilities;
- Masterplan should include a site wide infrastructure strategy.

A ‘Public Consultation Report’ was produced following the consultation. This sets out a summary of the consultation process, the comments received and the Council’s response including an explanation of how the masterplan has been able to take account of the views and comment received. The report acknowledges that it is not possible to address all of the matters raised; the Council considers it has responded as positively as possible.

A copy of the consultation report is available on the Council’s website.

1.0 SETTING THE SCENE

1.5 SITE CONTEXT

1.5.1 ACCESSIBILITY

The site lies adjacent to the borough’s strategic road network. The A6119 forms the southern boundary of the site. Figure 3 illustrates the sites proximity to the wider strategic road network.

The A6119 (Yew Tree Drive/Ramsgreave Drive) is a dual carriageway with two lanes in each direction. The A6119 runs southeast and connects to Junction 6 of the M65, which is approximately 4km from the site, and runs westwards to meet the A667. The A667 leads west to Preston and Junction 31 of the M6 and extends southwards to Blackburn town centre. Wider links connect to the M60 and into Manchester via the M61.

The nearest train station to the site is Ramsgreave & Wilpshire station, which is located approximately 1.5 kilometres from the access road to the eastern site.

It likely that residents will use Blackburn train station to travel by rail, which can be accessed via a 10 minute bus journey from the site and a short walk from Blackburn Centre.

Pedestrian

Existing foot ways including public rights of way and street lighting on the surrounding roads, including Whinney Lane, Yew Tree Drive, Ramsgreave Drive, and Lammack Road, ensure that the site has good connectivity on foot to the local services which are currently serving the established areas.

Cycle

Mellor Lane, located approximately 500m north of the site, is an on-road regional cycle route, which runs eastwards to Ramsgreave and Wilpshire railway station. National Cycle Network Route 6 connects through the centre of Blackburn, this route serves the town centre via the Leeds & Liverpool canal tow path from the East running through town and out to Witton Country Park.



FIGURE 3
PLAN ILLUSTRATING STRATEGIC MOVEMENT NETWORK

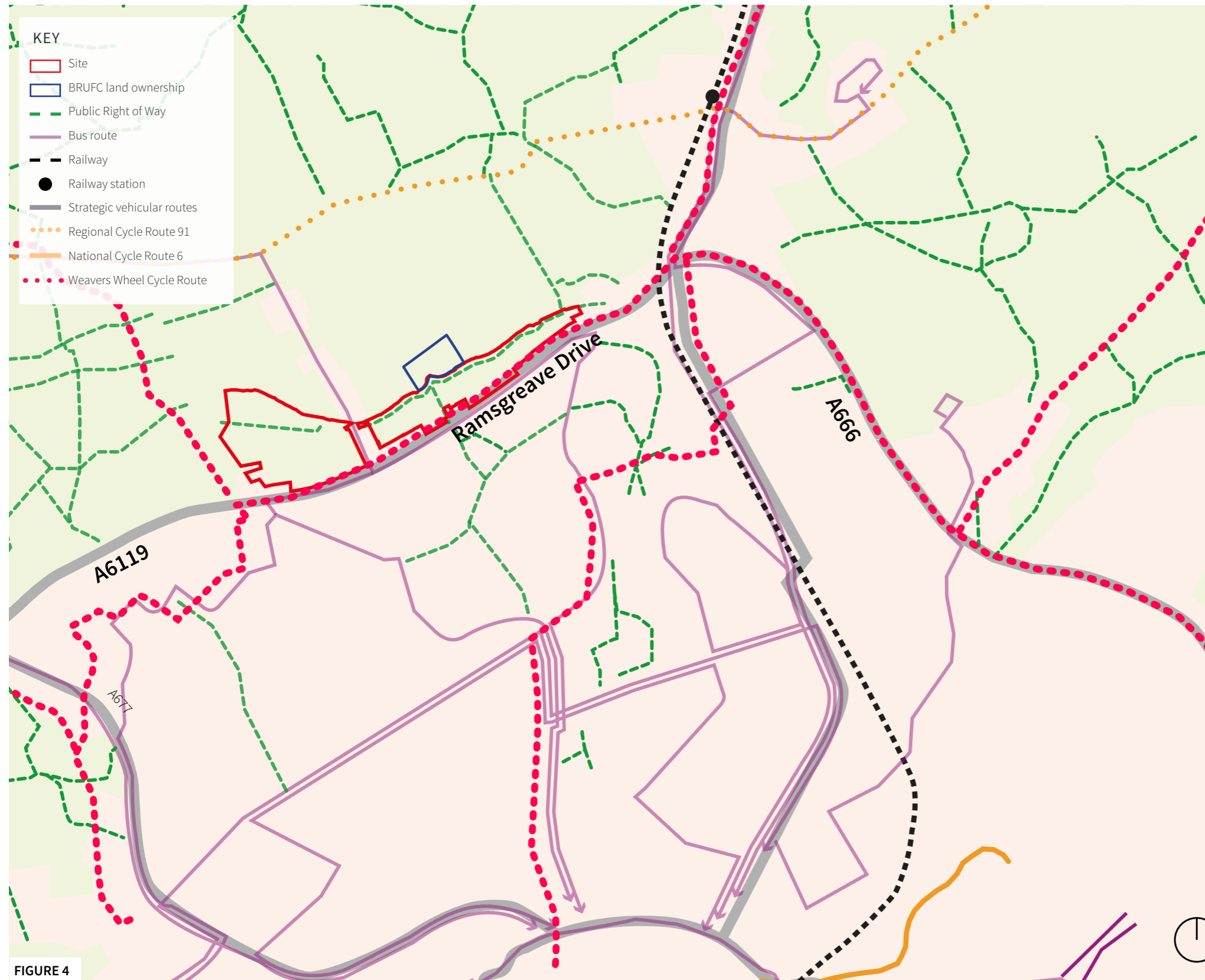


FIGURE 4

PLAN ILLUSTRATING EXISTING MOVEMENT NETWORK

The site has accessibility onto the national cycle route via the strategic cycle network. The Weavers Wheel Circular Route connects to the NCN Route 6 at several sections, notably at Witton Park. The masterplan will provide cycle connections to the surrounding existing routes.

Bus

Bus services connect the site to Blackburn town centre. Wider connections from the central bus interchange in Blackburn regularly link to Preston, Bolton, Burnley and the outlying villages.

Rail

Ramsgreave and Wilpshire railway station is the nearest station to the site. The station is currently served by a daily hourly service northbound to Clitheroe and southbound to Blackburn and Manchester Victoria, with extra trains running during peak hours.

Planning for the future, the Clitheroe to Manchester Victoria Line is being upgraded and will ultimately provide a 30 minute frequency for most of the day as opposed to hourly. This improvement will significantly approve the site/resident connectivity.

Conclusion

The site is easily accessible through a variety of different modes of transport. There is an opportunity to integrate the site with the surrounding established road, public transport, public footpath and cycle networks.

1.0 SETTING THE SCENE

1.5 SITE CONTEXT

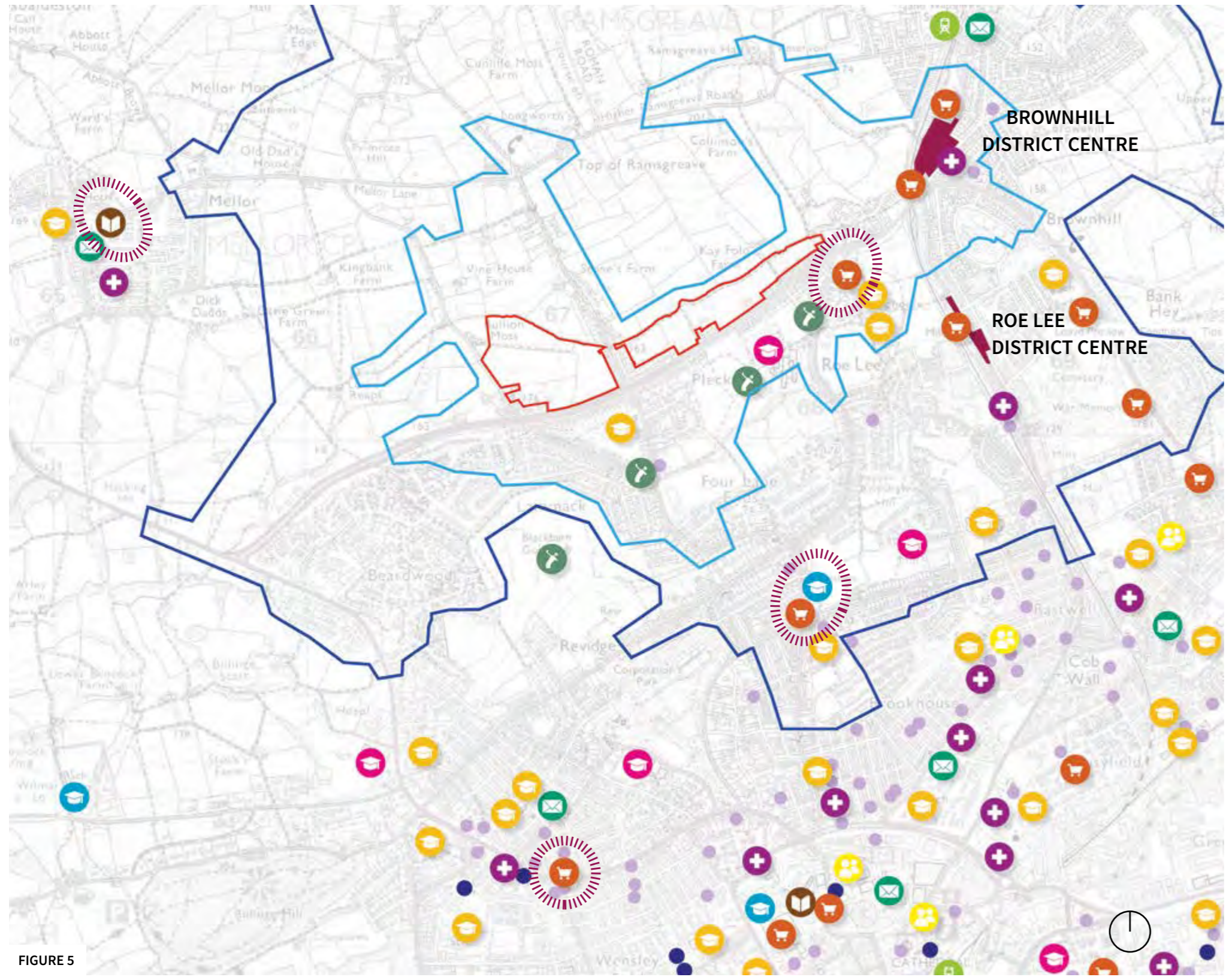


FIGURE 5
PLAN ILLUSTRATING WALKING DISTANCES TO LOCAL FACILITIES

1.5.2 LOCAL FACILITIES

The site benefits from the proximity of a wide range of established community facilities within the surrounding area. Figure 5 indicates the distribution of facilities within the surrounding area.

The Brownhill District Centre is within a 12 minute walk and the Roe Lee District centre is within a 12-25 minute walk of the development site. Both District Centre's offer access to a range of services and facilities including GP's surgeries.

It is possible to access: primary and secondary schools; a chemist and a GP's surgery; several recreational facilities, in addition to clusters of local shops in Pleckgate Road or Four Lane Ends.

- KEY**
- Site
 - 1km/12 mins walking distance
 - 2km/25 mins walking distance
 - Library
 - Community centre
 - Retail
 - Sport Facilities
 - Post office
 - Nursery school
 - Place of worship
 - Primary school
 - Secondary School
 - College
 - GP's surgery
 - Local neighbourhood centre
 - Train station
 - District Centres

1.0 SETTING THE SCENE

1.5 SITE CONTEXT

1.5.3 LANDSCAPE CHARACTER

The Lancashire County Council Landscape Character Assessment (2000) defines the landscape type as ‘Farmed Ridges’, and the character area as ‘Mellor Ridge’. The character areas identified within the study are illustrated in figure 7.

- KEY**
- Site
 - BRUFC land ownership
 - 1 Mellor Ridge
 - 2 Samlesbury- Withnall Fold
 - 3 Urban Area

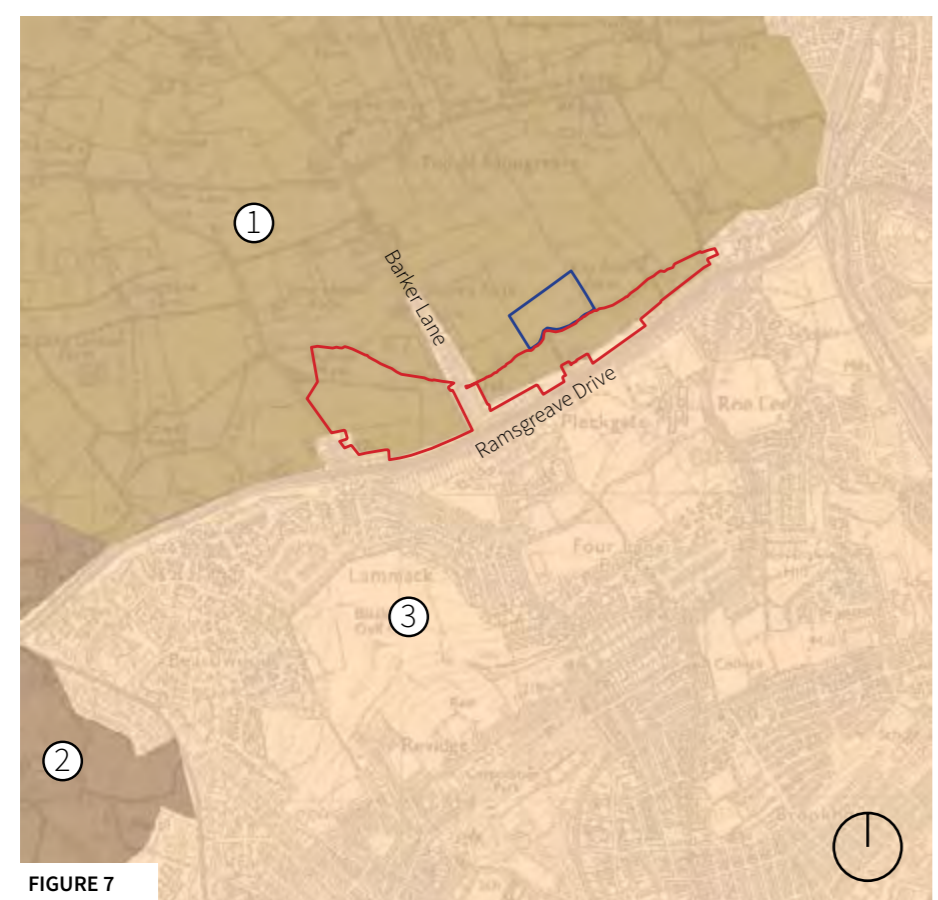


FIGURE 7
PLAN ILLUSTRATING THE SURROUNDING AND SITE CHARACTER AREAS



FIGURE 6
PHOTOGRAPH OF THE SITE IN THE FOREGROUND WITH MELLOR RIDGE BEHIND

Farmed Ridges are described as follows:

“Intensive farming practices, mostly concerned with the grazing of beef, dairy cattle and sheep limit the nature conservation value of the area. The ridges themselves support a mosaic of mixed farmland and woodland which provides a textural backdrop to the surrounding lowlands.”

The landscape character one side of the ridge may be totally different from the character on the other, despite their proximity to each other. The local vernacular is clustered stone built villages with scattered outlying cottages and farmsteads strung out along local roads, but more recent ribbon development and new houses display an incongruous mix of materials. There is a good network of footpaths, parking and picnic spots with views over the surrounding lowlands”.

Mellor Ridge is described as follows:

“A prominent lowland ridge which forms a south-western gritstone extension to Pendle Hill, separating the rural Ribble Valley from the industrial Calder Valley. It is under pressure for urban development, but despite this influence it appears rural in character from the surrounding valleys and provides an important buffer between the intensely urban landscape of Blackburn and the rural landscapes associated with the Ribble Valley. The prominent ridge line is viewed from the busy A59 and M65 transport routes to the north and south respectively. There are also outstanding views from the ridge itself across the Ribble Valley to the north, over Whalley Abbey at its eastern end and across Blackburn to the south. Built development has taken advantage of these views and Mellor is sited on top of the ridge where it enjoys long views across the adjacent lowlands.”

The Mellor Ridge, which runs in a east west direction, has been cut off from the larger mass of Pendle Hill by the River Calder. The Calder to the east, the Ribble to the north and the Darwen and Hindburn Brook to the south, have shaped the ridge by fluvial and glacial processes”.

SUMMARY

The site is located at the southern foot of the Mellor Ridge, but overlaps in part with the fringes of the adjacent urban area. Whilst the site displays characteristics of the Farmed Ridge Landscape Type and the Mellor Ridge Character Area, it also shows some suburban elements, particularly along the A6119 edge and rising up the hill along Barker Lane. A more detailed, site level landscape character analysis is included within section 1.7 of this document, which explores the rural/suburban juxtaposition in more detail.

1.0 SETTING THE SCENE

1.5 SITE CONTEXT

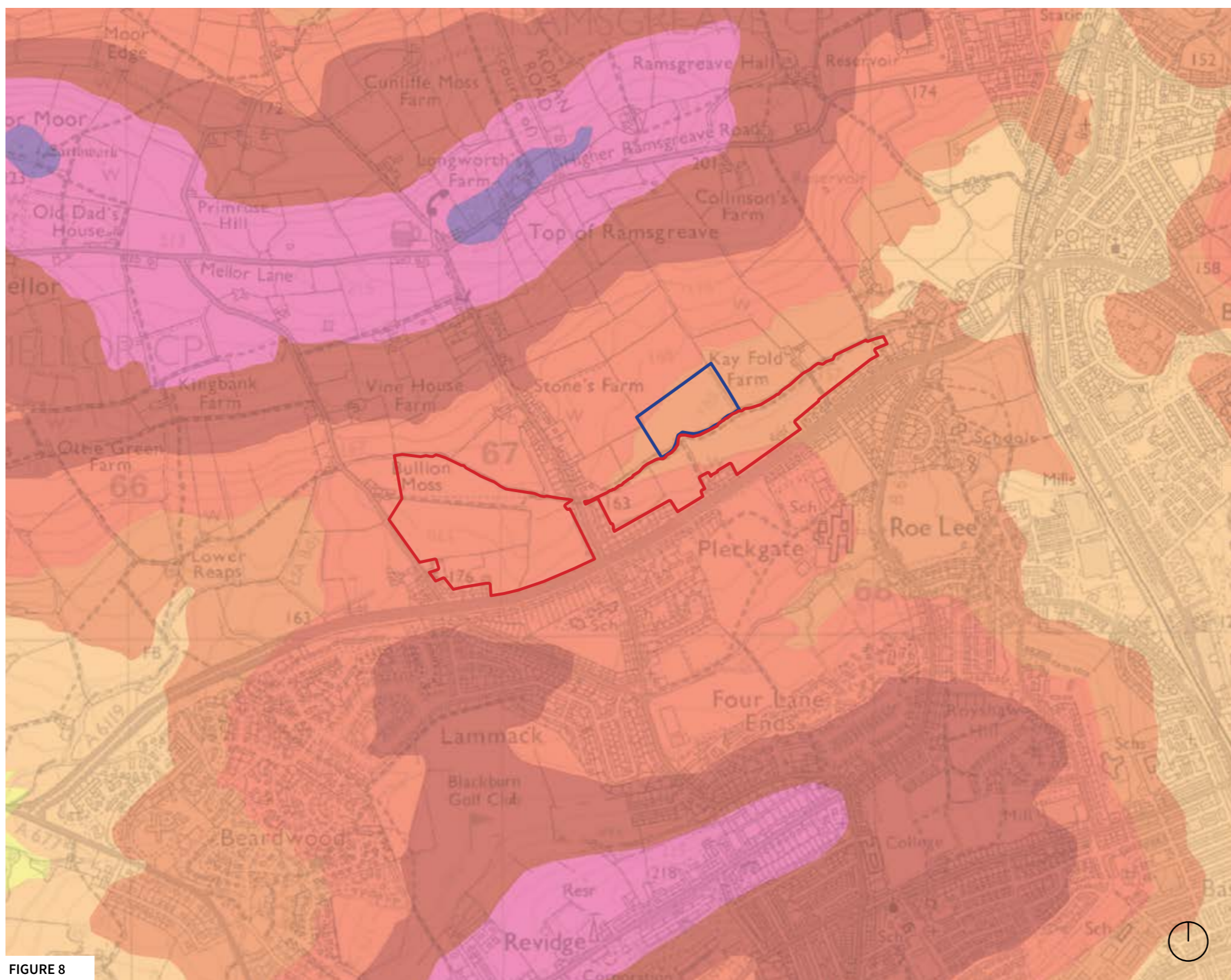


FIGURE 8

PLAN ILLUSTRATING TOPOGRAPHY WITHIN THE SITE AND THE SURROUNDING AREA

1.5.4 TOPOGRAPHY

Figure 8 illustrates the wider topography surrounding the site. An understanding of topography within the wider site context is key to understanding the potential visibility of the site, the site landscape character, and the visual and physical relationship of the site to the surrounding urban and rural context.

The site is defined by its topographical position. Strategically the site sits within a valley between Mellor Ridge to the north and Revidge ridge to the south. This indicates that there are potential views from higher ground within the rural landscape to the north, and from within the urban area to the south (indicated by the purple and blue shading). There are also elevated ground levels with potential views towards the site from the east (light yellow shading). Higher ground levels to the north and to the south will visually contain the site, and prevent more long range views towards the site.

Within the site itself, ground levels fall gradually on a roughly east-west axis. Ground levels within the site will be explored in more detail in section 1.6 (page 36) and section 2.6 (page 55) of this document.

KEY

- Site
- BRUFC land ownership
- 100
- 100-120
- 120-140
- 140-160
- 160-180
- 280-200
- 200-220
- 220-240

1.0 SETTING THE SCENE

1.5 SITE CONTEXT

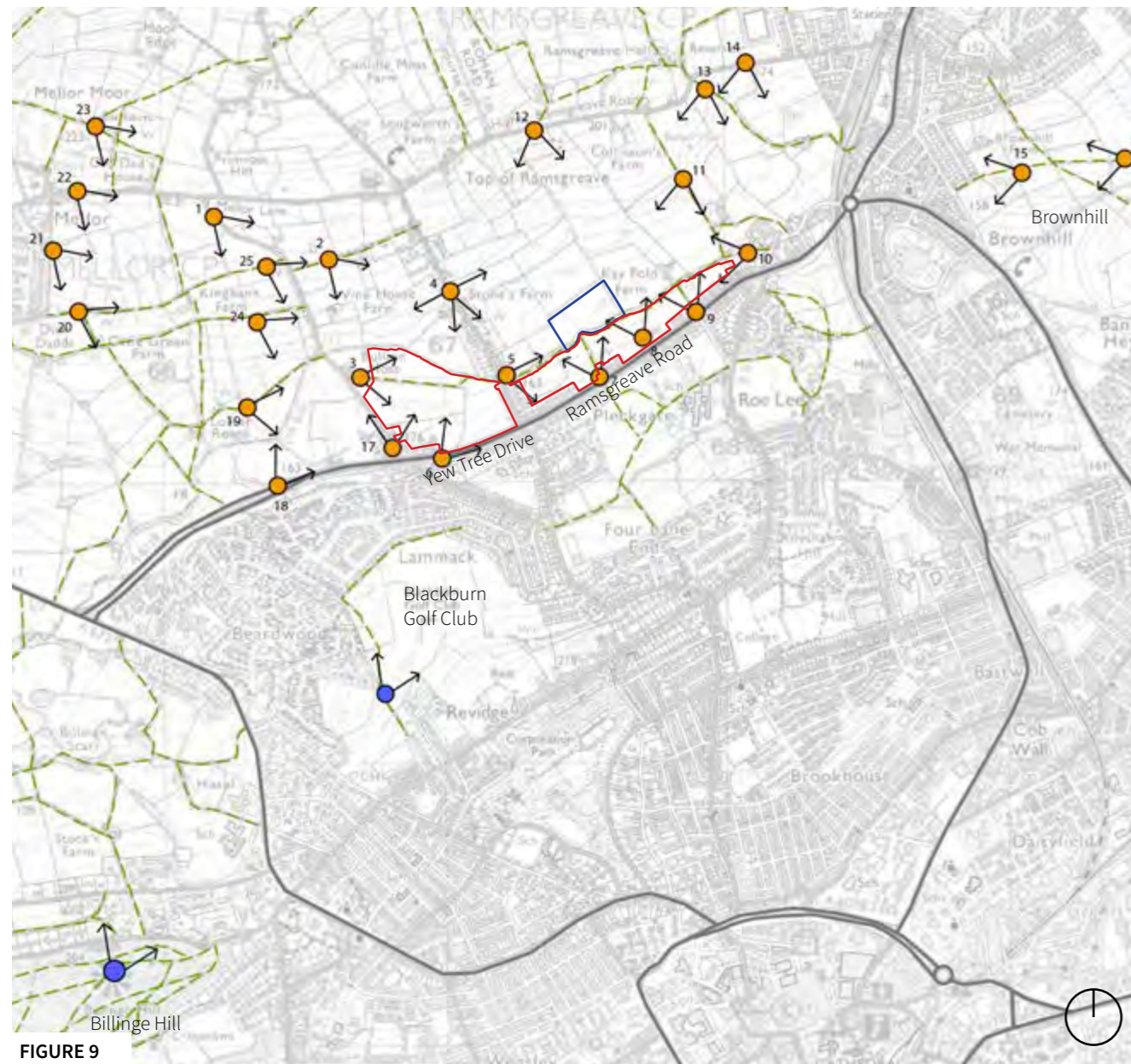


FIGURE 9
PLAN ILLUSTRATING POSITION OF VIEWS

1.5.5 SITE VISIBILITY

The topography within the site and surrounding context impacts significantly on the overall visibility of the site. The site sits on the northern fringes of the town centre, however, views from the south are limited due to the general fall in ground levels. There is a significant rise in ground levels on Billinge Hill to the south-west, however dense mature tree cover prevents views. Similarly, dense tree cover along the perimeter of Blackburn Golf Club prevents long range views towards the site from the south. Ground levels rise steeply to the north of the site, allowing views from the local road and public footpath network. Ground levels also rise sharply to the east, allowing long range views from Brownhill. The identified key views are illustrated on figure 9

Due to the separation of the site into two distinct land parcels, separated by development and tree cover, there are limited opportunities for views of the whole site. Long range views of the majority of the site are potentially available from Brownhill to the east, and Ramsgrave Road to the north. Panoramic views of the eastern half of the site are available from the rugby club (View 8). Views of the western parcel are more fragmented, due to strong hedge lines across the site.

Key views of the site were identified through a process of desk-top and on-site survey. In terms of assessing the baseline visual sensitivity, key factors that have been considered are the type of view and the likely numbers of viewers (the visual receptors).

1.5.6 VISUAL SENSITIVITY

Consideration has been given to the overall sensitivity of each of the key viewpoints. A detailed methodology is located in Appendix A (page 66) along with photographs of each of the key viewpoints. The conclusions of the assessment are summarised below:

- All views were assessed as ‘low’ in terms of their value, due to no views from landscapes of national, regional or district importance.
- The majority of views (15) were assessed as ‘high’ in terms of their susceptibility to change. Only 3 views were assessed as ‘low’ in terms of their susceptibility to change (Views 6, 8 and 18).
- No views were assessed as ‘high’ in terms of their overall sensitivity.

The majority of views were assessed as medium-medium/low, with only 3 views assessed as ‘low’ (Views 6, 8 and 18). Any potential impacts on the key viewpoints will be a key consideration when developing the masterplan proposals to ensure that any visual effects are reduced, mitigated against or, where possible, avoided. Particular consideration will be given to the most sensitive identified viewpoints.

1.0 SETTING THE SCENE

1.5 SITE CONTEXT

1.5.7 PUBLIC OPEN SPACE

Several accessible open spaces are in close proximity to the masterplan area.

Figure 10 illustrates that within 2km/25mins walk from the site there are accessible parks/key open spaces at: Pleckgate Road (which includes publicly accessible playing pitches), Corporation Park and Roe Lee Park.

The site is well placed to benefit from a variety of different types of public open spaces. The countryside to the north of the site is also very accessible from an existing network of public footpaths.

The potential exists to improve the open space offer by utilising land within the wider ownership of the Rugby Club, located to the north of the existing site area. Overall the site is well served by a variety of public open spaces.

- KEY**
- Site
 - BRUFC land ownership
 - 1km/12 mins walking distance
 - 2km/25 mins walking distance
 - Parks and public gardens
 - Provision for children and young people
 - Natural and semi-natural green spaces
 - Allotments
 - Cemeteries
 - Accessible countryside in the urban fringe
 - Formal outdoor recreation (Inc. school grounds)
 - Golf course

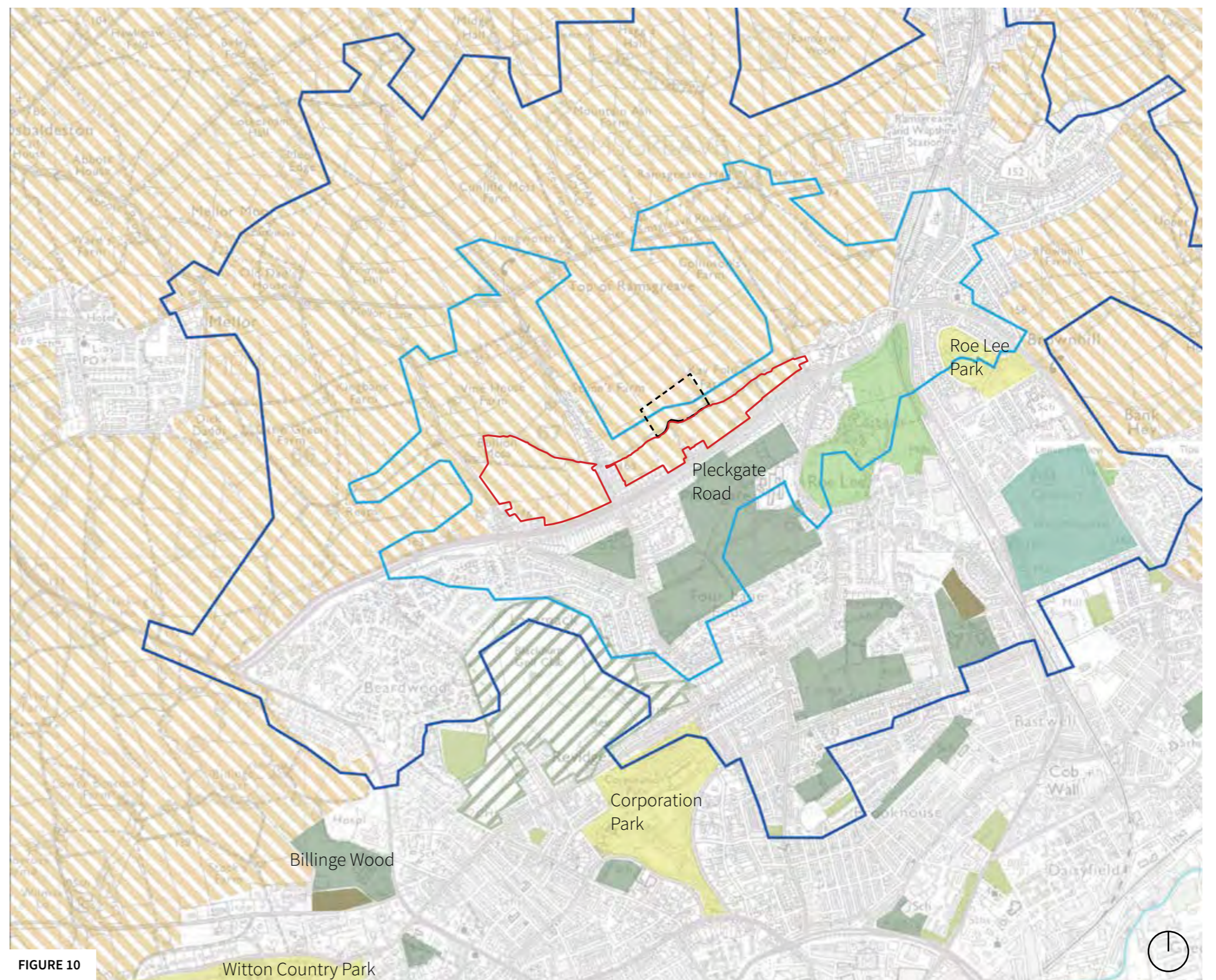


FIGURE 10 PLAN ILLUSTRATING EXISTING PUBLIC OPEN SPACES WITHIN THE LOCAL AREA

1.0 SETTING THE SCENE

1.5 SITE CONTEXT

1.5.8 HISTORICAL EVOLUTION

The historical evolution of Blackburn and the site reveals some interesting facts regarding the development of the town and its relationship with the surrounding countryside.



FIGURE 11

1890

The centre of Blackburn has developed and the Victorian character is starting to emerge. The site at this time is very much rural in character and is surrounded by countryside and small farmsteads.



FIGURE 12

1929

By this time the eastern end of Ramsgreave Drive has been built and houses in Pleckgate and Brownhill.



FIGURE 13

1955

By now Yew Tree Drive and Ramsgreave Drive has been turned into a substantial ring road around the town, but it still at this point passes through rural areas, with pockets of countryside both sides of the road as it passes the site boundary.



FIGURE 14

1971

Significant infill development emerges south of Ramsgreave Drive and Yew Tree Drive. The original Pleckgate High School has been built west of Pleckgate Road.

1.0 SETTING THE SCENE

1.6 TOWNSCAPE STUDY

1.6.1 PURPOSE OF TOWNSCAPE STUDY

This townscape study has been undertaken in order to gain a thorough understanding of the existing characteristics of Blackburn, the surrounding settlements and how they could potentially relate to the site and inform the emerging design proposals.

The historical evolution and distinctive features of the site have been highlighted to help build a picture of the shape and form of the site, how it relates to its context and what aspects may drive a scheme in developing a sensitive and appropriate townscape for the development.

Following this, the first step has been to identify the overall residential character areas of Blackburn and its surrounding settlements. Several study areas have then been selected to be analysed in more detail. These have been chosen on the basis that they share similarities with the existing characteristics or conditions of the site. Deeper analysis of these particular areas enables the design team to develop an holistic understanding of the urban and architectural features which influence the character and townscape of these places. The study areas range from the town to the rural. The initial identification of surrounding urban character areas is identified in Figure 15.

KEY

- Site
- BRUFC land ownership
- City historic core
- C19th Terraced housing
- C19th Detached and semi-detached housing
- Early C20th Semi-detached
- Mid C20th in-fill housing
- Late C20th suburb
- Rural
- Character areas

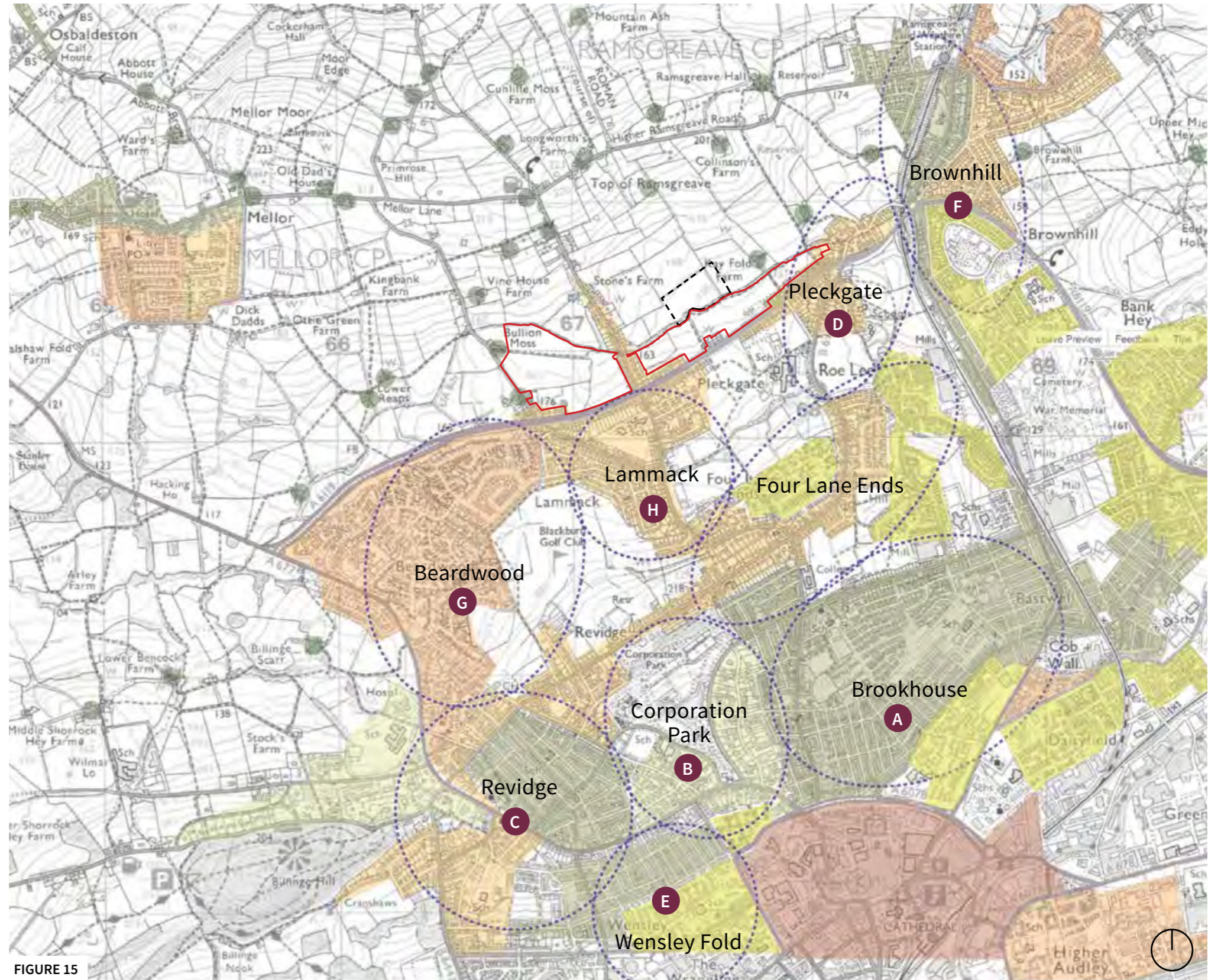


FIGURE 15

PLAN ILLUSTRATING LOCATION OF SURROUNDING URBAN CHARACTER AREAS STUDIED (TO BE READ IN CONJUNCTION WITH PHOTOS ON PAGE 21)

1.0 SETTING THE SCENE

1.6 TOWNSCAPE STUDY

To conclude, the study puts forward recommendations and outlines our approach in developing a townscape strategy.

Blackburn consists of several different periods of development which contribute to a townscape of varying quality.

The housing areas include Victorian terraces which spread out from the town centre in narrow street and a regular grid layout (A).

During the same period of development, a neighbourhood of very large grand houses set in large grounds was established around the new public Corporation Park (B).

Later in the C19 and early C20th suburban housing developments formed a concentric ring around the town. Properties are set back from the tree lined streets and often arranged in a geometric layout (C,D).

During the second half of the C20th slum clearance and de-industrialisation created space for in-fill housing development closer to the centre. These are often characterised by properties turning their backs to the street, within inward looking estates (E).

Late C20th and early C21st suburban developments are characterised by low-rise, low density properties arranged in irregular cul-de-sacs (F,G).

The hamlets and villages surrounding the urban centre are characterised by cottages and farm houses with ancillary buildings, often addressing the street directly or arranged in courtyards (H).



PHOTOS ABOVE ARE EXAMPLES OF THE SURROUNDING URBAN CHARACTER AREAS STUDIED (TO BE READ IN CONJUNCTION WITH FIGURE 15)

1.0 SETTING THE SCENE

1.6 TOWNSCAPE STUDY

1.6.2 SURROUNDING SETTLEMENTS

Figure 16 indicates that the site lies along a strategic line between the urban and rural edge. Looking beyond the urban core and out to include the surrounding countryside, the study has found that some of the surrounding villages provide useful evidence of a vernacular character unique to this area.

Some settlements have already expanded beyond the urban edge and embody both urban and rural qualities, these too have been highlighted as useful to include in the analysis to understand how these places evolved and whether they have developed a unique character.

Appendix B (page 81) presents a townscape study of several areas from the urban to the rural, they include:

- Urban: Corporation Park Conservation Area
- Urban/ Rural edge: Brownhill and Wilpshire
- Rural Village: Mellor and Mellor Brook
- Rural Hamlet/ Farmstead: Showley Fold

The study has focused on analysing particular layout and architectural features:

- Scale and urban grain
- Topography and landscape
- Gateways and landmarks
- Boundaries and parking
- Materiality and architectural detailing

Brownhill and Wilpshire share many similarities so have therefore been analysed together. This is the case also for Mellor and Mellor Brook. Showley Fold, although only a very small collection of buildings have been analysed to gain a deeper understanding of rural housing typologies and could be a useful precedent in very particular parts of the site. Aerial photographs showing the relationship between urban form and landscape are provided within page 34.

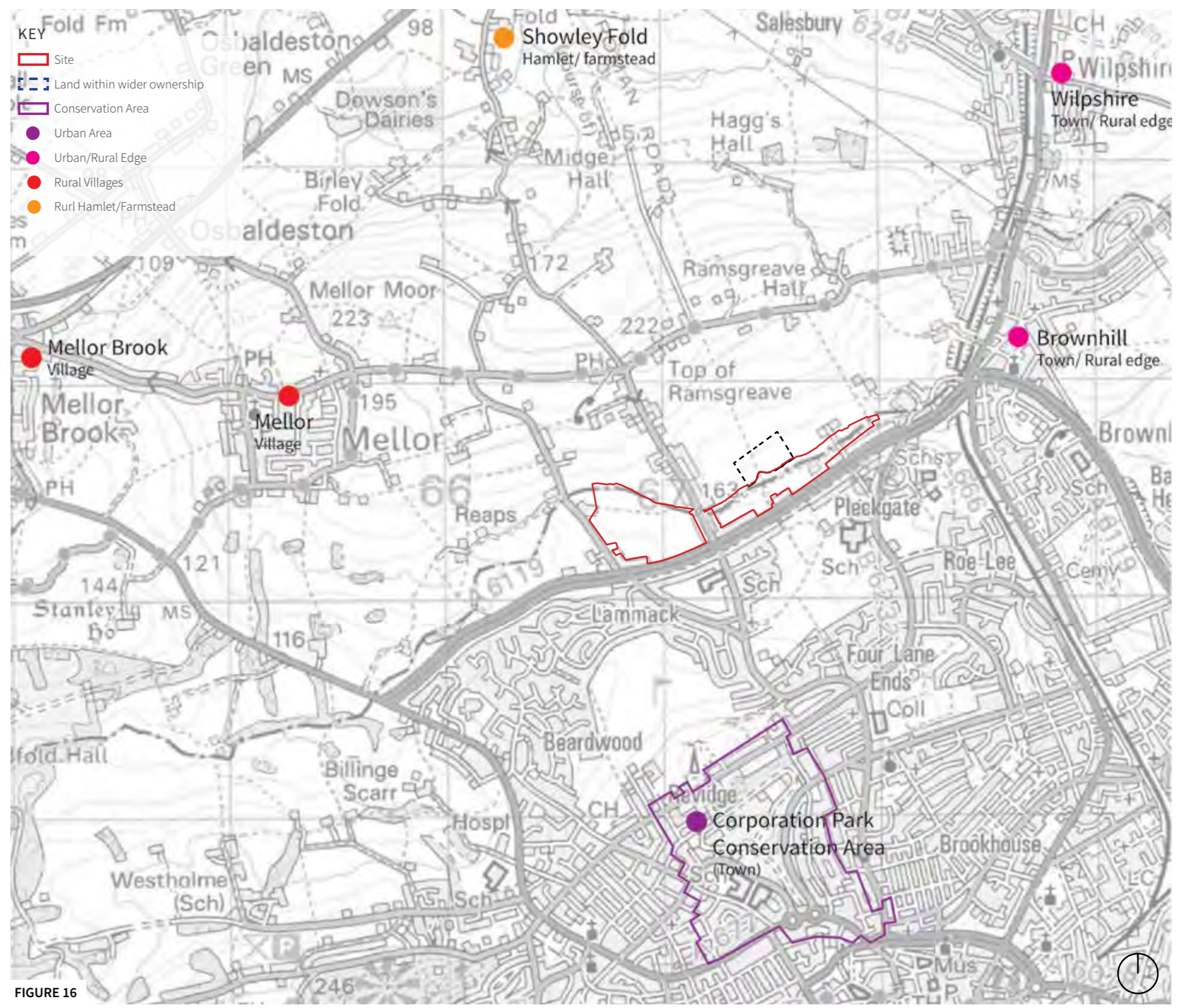


FIGURE 16
PLAN ILLUSTRATING SURROUNDING SETTLEMENTS

1.0 SETTING THE SCENE

1.6 TOWNSCAPE STUDY



THE CLUSTER OF BUILDINGS ARE APPROACHED BY A RURAL LANE, AND ARE SURROUNDED BY FIELDS.



A CORNER BUILDING MARKS THE ENTRANCE TO THE FARMSTEAD AT THE JUNCTION WITH THE RURAL LANE

This study has highlighted several opportunities to create a sensitive townscape for the site, in order that it sits comfortably within its surroundings and doesn't harm the existing qualities and distinctiveness of the area.

At this stage, the following recommendations have been put forward for consideration in the design of any masterplan proposals.

- The development must relate to Yew Tree Drive and Ramsgrave Drive, both visually and physically, so that the scheme integrates fully with the wider urban Blackburn area.
- The development must be sensitive to the northern rural boundary of the site by retaining existing significant landscape features and providing an appropriate setting for them and the new development.
- A sensitive response to the topography of the site must drive the scheme layout and the position and line of development. A variety of solutions may be appropriate
- A clear distinction between the urban and rural edge will help to maintain a key characteristic of Blackburn.
- The inclusion of a series of semi-rural courtyards along the interface with the surrounding open countryside.
- The provision of stone boundary walls and hedges around the edges of more outward looking open space and open countryside.
- The layout of the development should create the potential for views of the wider countryside to the north.

- The use of village lanes with a variety in housing types and density.
- The provision of key marker buildings which are offset from the building line to suggest and define incidental spaces.
- The materiality and detailing of the scheme should reflect the variety of materials within the Blackburn area, particularly the common use of brick and render and reflect the surrounding traditional vernacular of the town of Blackburn and rural areas.

Examples of the semi-rural characteristics can be found within the surrounding villages of Mellor and Mellorbrook, a townscape study of these villages are provided within Appendix B (page 80, 89, 90, 91 of this document).

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.1 HIGHWAYS AND TRANSPORT CONSIDERATIONS

A Transport Assessment (TA) and Residential Travel Plan Framework have been prepared to inform the preparation of the masterplan based on an assumption of 300 units being delivered on the western parcel (between Whinney Lane and Lammack Road) and 275 on the eastern site.

The transport consultants who prepared the documents have worked with the local highway authority and with consideration of consultation feedback in the drafting of their reports.

The purpose of the TA is to evaluate and inform the transport and highway implications of the proposed development including an assessment of the travel needs of various modes including walking/cycling/public transport and car; connectivity to surrounding services and facilities; potential access points to the site for car and non-car modes, and; the capacity of nearby road junctions. Additionally the TA will assess how the travel demand can be met in a manner which ensures access by all modes, maximises access to the site by sustainable modes, protects the safety of all

road users and sets out a programme of measures to achieve all above. The TA has identified vehicle and pedestrian/cycle access points. These are shown on the masterplan framework in Section 2.

The rationale for the TA's approach to access is to:

- Enable separate parcels of land to be developed independently;
- Provide permeability for pedestrians and cyclists by providing appropriate crossing facilities at vehicular access points and by providing additional suitable non-car access points. The intention is to provide convenient connections to existing rights of way and cycle routes (principally the Weavers Wheel cycle network);
- Alter the nature of the dual carriageway (A6119) to create a more residential 'sense of place', and
- Avoid the potential in scheme layout for inappropriate routing including rat-running.

National planning policy requires all development that generates a significant amount of transport movement to provide a Travel Plan. This is reinforced in the Council's local plan Policy 10. Accessibility and Transport. The aims of the Travel Plan are to:

- Encourage residents and visitors to use alternative and more sustainable means of transport to the private car;
- Increase awareness of the advantages and potential for travel by more environmentally friendly and healthy modes, and;
- Introduce a package of management measures that will enable travel by modes of transport other than the private car.

Full details of the TA assessment and Travel Plan recommendations for the development of this site are provided in the following documents which will be published as supporting evidence and should be read alongside the adopted masterplan:

- Proposed residential development : Yew Tree Drive and Ramsgreave Drive, Blackburn with Darwen Borough Council, Transport assessment, September 2016
- Proposed residential development : Yew Tree Drive and Ramsgreave Drive, Blackburn with Darwen Borough Council Residential Travel Plan Framework September 2016
- Further details on the developer's obligations relating to the delivery of the measures identified in the TA and Travel Plan are set out in Section 3:Delivery and the accompanying Infrastructure and Delivery Plan.

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS



ABOVE: PHOTOGRAPH OF RAMSGREAVE DRIVE

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.2 ECOLOGY

A number of ecological assessments have been undertaken across the site to provide an overview of any likely ecological issues and provide recommendations for mitigation and/or further survey. The survey results are, summarised within Figure 17, and broadly indicates the following:

- There are no statutory or proposed statutory sites of importance for nature conservation within 2km of the site boundaries or within the site boundaries. However parts of the site are located between Whinney Lane and Lammack Road and are located within an Ecology Standing Advice Consultation Zone.
- A principal habitat within the area is improved grassland, with areas of marsh/marshy grassland, buildings and hardstanding and running water. Field boundaries include species-poor (defunct) broadleaved hedgerow and a dry stone wall..
- The most ecologically sensitive areas within the site include established trees and hedgerows and water bodies across the site.

It is considered that the habitats within and adjacent to the site may have the potential to support:

- Common reptile species within the areas of open grassland;
- Nesting birds within areas of trees and hedgerows;
- Roosting bats within mature trees (if present); and
- Water voles and great crested newts within water bodies (i.e. within the ditch in the site boundaries and the ditch to the north of the site), if they are noted to contain water for the majority or part of the year.

A great crested newt survey was undertaken on the site to the west of Barker Lane confirming the absence of breeding great crested newt from the water bodies

surveyed. Therefore great crested newts do not represent an ecological constraint for this part of the site.

A Bat Activity Transect Survey report was undertaken for the site to the west of Barker Lane, no confirmed bat roosts have been recorded within the survey area, however a pre-construction ground inspection of two structures within the site is recommended. Bat activity was recorded within the site therefore it is recommended that external lighting within the development is minimised as far as practicable and features of interest should be enhanced and retained.

Figure 17 highlights the landscape features of interest and potential wildlife habitats within the site. The adjacent key describes the features and ranks the ecological value from low to high.

Summary

The findings and conclusions of the habitat surveys have been incorporated within the design framework plan which has been designed to retain and enhance landscape features and areas of high ecological value as part of an overall green space strategy, in order to achieve a well designed layout.

On the basis of the survey work completed to date, it is not considered that there are any ecological issues which would prevent the development of the site or which cannot be mitigated or addressed by planning condition/contribution. However more detailed ecological assessments will be required to support individual planning applications.

Key

1. An area of self-seeded neutral grassland with occasional tall-ruderal and scrub vegetation. Low-moderate ecological value. Retain if possible but not essential.
2. An extension of area 1. Comprising the same species and vegetation structure, situated on a sloping ground, with a semi-defunct hedgerow at the top of the bank along the site boundary. Retain if possible, especially the hedgerow, which should be consolidated.
3. The main body of semi-natural neutral grassland linked to areas 1 and 2 comprising mainly common rank grassland species with occasional tall-ruderal vegetation and scattered scrub. No species or vegetation communities of particular importance likely to occur. Low-moderate ecological value. Retain if possible but not essential.
4. The stream that crosses the site and adjacent bankside vegetation. The vegetation is not especially species-rich but is structurally diverse with a soft bank and open, flowing. Suitable for use by water voles though no conclusive signs found. Moderate to high ecological value. Retain watercourse plus a minimum stand off of 5m either side of the bank.
5. A relatively small area of unimproved acid grassland and marsh with a number of typical vascular plants and mosses, including sphagnum, present. Potentially high ecological value that will require confirmation from further survey.
6. An area of rank neutral grassland and scrub with some large patches of tall-ruderal, growing on a relatively high, steep embankment. Not particularly species but with occasional damp flushes. Low moderate ecological value. Retain if possible.
7. Rank grassland separated by an access track. Low ecological value.
8. A section of stream which feeds into the main stream that crosses the site. The bankside is steep but well vegetated. There is a mature hedgerow to the rear and a strip of rank neutral grassland and scrub. Moderate to high ecological value. Retain watercourse, plus a 5m margin either side, including hedgerow if possible.
9. Species-poor rank neutral grassland and scrub. Low ecological value – no need to retain.
10. An area of scrub and ranks grassland. Not especially important in botanical terms but good bird breeding habitat. Moderate ecological value. Retain if possible.
11. A patch of scrub and ranks grassland along the site boundary. Moderate ecological value, retain if possible but not essential.
12. A section of the stream that crosses the site containing mature hedgerow and well developed vegetation. Moderate – high ecological value. Retain stream plus a 5m margin either side of the bank, including hedgerow.
13. The existing club house and associated buildings. Low to moderate potential for bat roosting. No need to retain.
14. As area 10 – moderate ecological value.
15. As area 11 – moderate ecological value.
16. A narrow band of neutral grassland, Low to moderate ecological value, no need to retain.
17. A section of stream that crosses the site. Contains aquatic flora suitable habitat for water vole and breeding birds. Moderate to high ecological value. Retain stream plus a 5m margin either side, avoid impact to adjacent mature trees.
18. Rush pasture dominated by rushes, may be used by overwintering birds. Low to moderate.
19. A steep bank with scrub and young trees. Moderate to high ecological value.
20. Semi-improved grassland, marshy grassland, and rush pasture grading into wetland habitat along the margins of the stream. Moderate to high ecological value. Retain as part of the river corridor
21. An area of swamp, marshy grassland and wetland habitat of moderate species diversity and relatively high ecological value. Retain as part of the river corridor
22. A mature hedgerow separating the Rugby Club from adjacent agricultural land. Moderate species diversity but comprising of mainly hawthorn with few other woody species. Moderate ecological value. Retain for screening and as a wildlife corridor.
23. An area of rough, semi-improved grassland bounded by a hedge that runs parallel to adjacent housing on Ramsgrave Drive. The hedge is mature but with numerous gaps. Moderate species diversity and low-moderate ecological value, retain hedgerow and adjacent rough grassland if possible.
24. The entrance to the site from Lammack Road comprising some dense areas of scrub, partially shading the stream, and a mosaic of rough grassland and tall-ruderal vegetation, dissected by an informal public footpath. Retain marginal vegetation wherever possible.
25. Nesting bird potential within trees and hedgerows.
26. Reptile potential within areas of open grassland.
27. Roosting bat potential within mature trees(if present). Defunct species-poor hedgerow.
28. GCN and Water Vole potential within water body.
29. Nesting bird potential within trees and hedgerows. Defunct species-poor hedgerow

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS



FIGURE 17

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.3 FLOOD RISK CONSTRAINTS AND DRAINAGE CONSIDERATIONS

A desk based review of potential flood risk constraints and drainage considerations has been carried out to inform the preparation of the masterplan. The key findings are summarised below. The full report has been published as supporting technical evidence and should be read alongside the masterplan document.

There is a small unnamed watercourse along the northern boundary of the site, flowing in an easterly direction. The topographic survey of the land to the east of Barker Lane indicates the banks of the watercourse are approximately 152m AOD and the banks rise steeply on either side of the watercourse. The development site slopes naturally towards this watercourse. There are also additional drains in the western area of the site between Whinney Lane and Barker Lane which discharge to the watercourse along the northern boundary.

Foul drainage will need to be installed to meet the requirements of Building Regulations and United Utilities.

1.7.4 FLUVIAL FLOOD RISK

The Environment Agency flood mapping (2015) suggests there is no risk of fluvial flooding to the site.

The Local Plan policy requirements for this site, Policy 16/2 – North Blackburn Development Site note that development must take into account:

- ‘8. Provision of SuDs and the incorporation of measures to control surface water run-off, flood risk from the northern watercourses and the consequences of blockages in the culvert.’

The desk based review concluded that the culverted section of the watercourse must be located downstream of the site. However, further, more detailed consideration of the culvert including an assessment of the need, if required, for any mitigation measures required to reduce the risk of flooding from the culvert would need to be completed as part of a full Flood Risk Assessment at planning application stage.

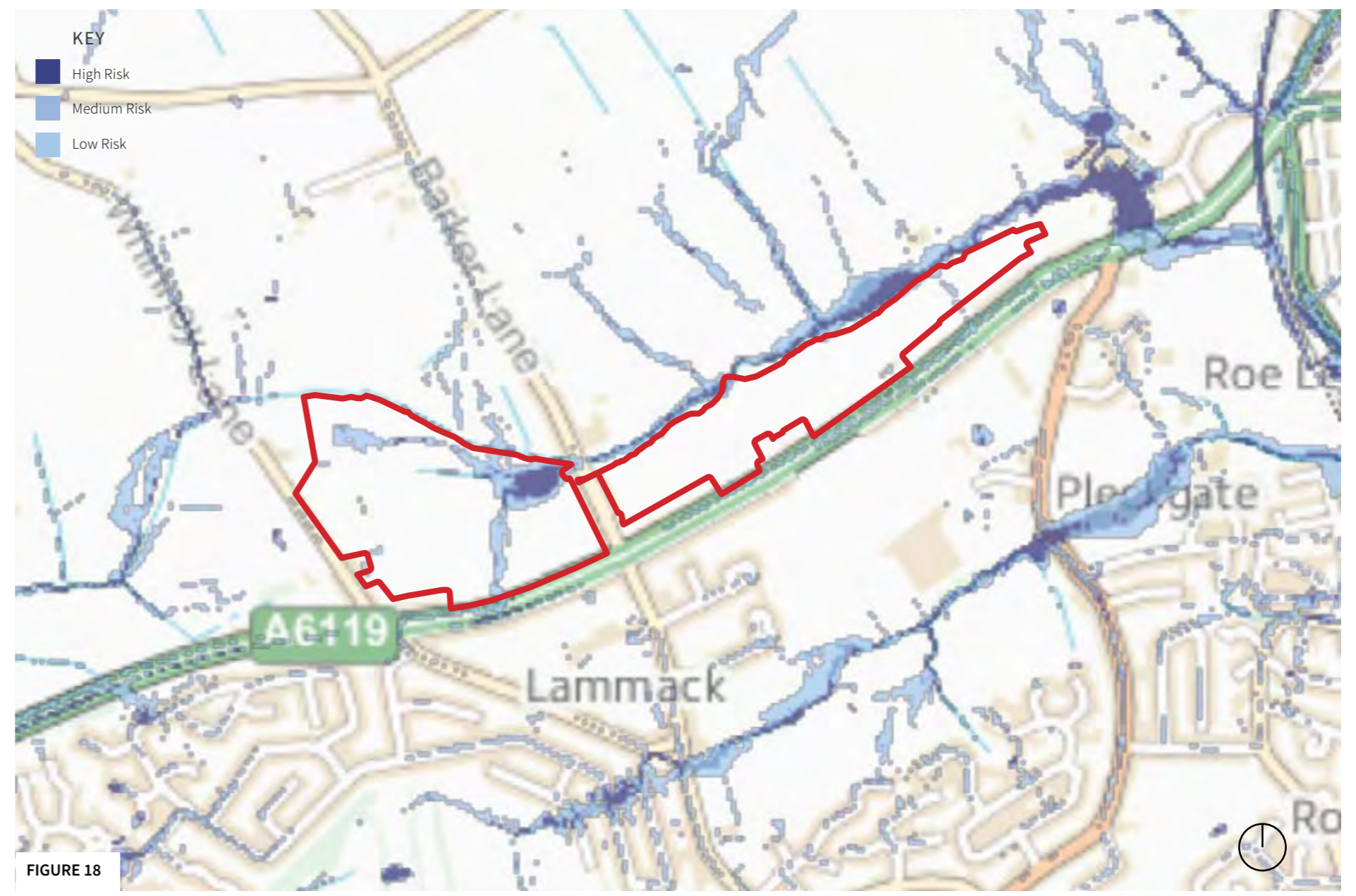


FIGURE 18

SURFACE WATER FLOOD RISK PLAN - COURTESY OF THE ENVIRONMENT AGENCY

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.5 SURFACE WATER FLOOD RISK

The Environment Agency online surface water flood map (2015) suggests that areas of the site may be at risk from surface water flooding. These are shown on Figure 18 and include:

- An area to the east of Barker Lane in the vicinity of the watercourse along the northern boundary of the site, and
- West of Barker Lane where several drains have been identified.

There is not considered to be any risk of flooding from the local reservoir, Ramsgreave Reservoir, or groundwater flooding.

1.7.6 SURFACE WATER DRAINAGE

A preliminary run-off assessment for the site has been completed to provide an indication of potential requirements for surface water management. This assessment has identified preliminary surface water storage requirements for the site and notes these could be provided as a singular or multiple surface storage facilities, SuDs features or underground storage. Details of the preliminary assessment, estimated storage volumes and the greenfield runoff rates which the surface water flows would be restricted to for discharges from the development are set out in the technical report. Essentially the developer will be required to limit the surface water runoff rates to equivalent runoff for storms of return period up to 1 in 100 years plus a 30% allowance for climate change.

The Council's preference is for an above ground SuDs solution incorporated into the green infrastructure framework. It is expected that this will be principally located and integrated within the green corridor along the northern boundary.

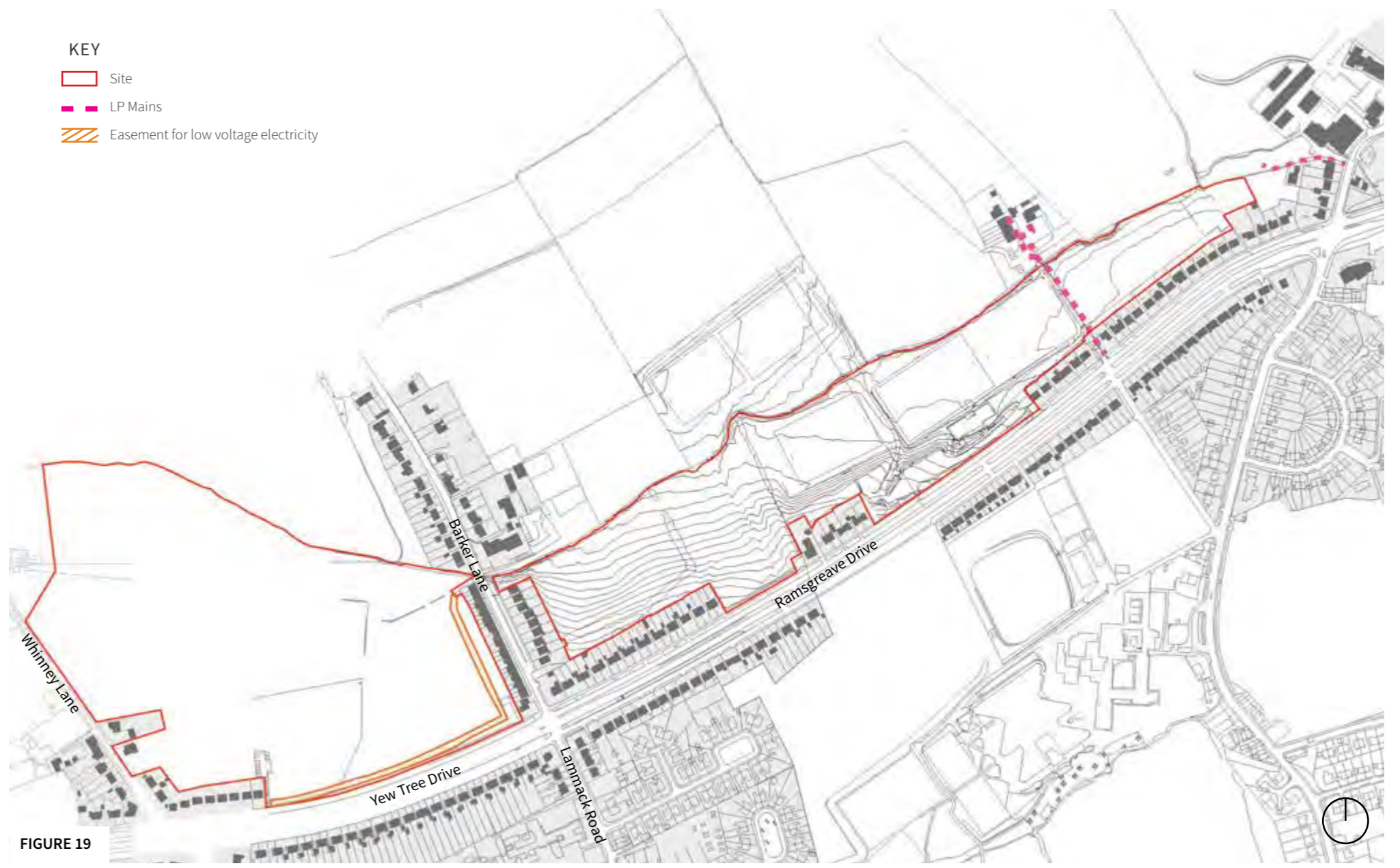


FIGURE 19

EXISTING UTILITIES PLAN

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.7 SITE DRAINAGE STRATEGY

As noted the site naturally drains towards the watercourse along the northern boundary. Additionally the runoff assessment took into account that the ground within the development site is classed as being heavy clay and therefore considered to be of low permeability.

Based on this preliminary assessment the report suggests that the drainage for the new development should be discharged to the watercourse at the greenfield runoff rates identified in the document.

It is recommended that the overall topography of the site should be kept similar (i.e. falling south to north) to existing to allow surface water flows to be naturally routed towards the watercourse. Any surface water runoff from roads through the development site should be routed into SuDs measures such as swales, which can additionally assist with removing any potential pollutants from runoff before it enters the watercourse.

It should be anticipated that the Environment Agency will require maintenance access to the watercourse. For rivers there is generally a requirement for an 8m buffer from the watercourse. This buffer would need to be excluded from the areas required for surface water storage.

1.7.8 MITIGATION MEASURES

The report recommends that the masterplan incorporates the following mitigation measures.

For the land to the west of Barker Lane, the main area of the site identified to be at risk of flooding:

- Locate properties outside the areas indicated to be at risk of surface water flooding, use the areas of risk as public open space/ recreational areas; refer to Figure 18.
- Properties should be located a safe distance from the watercourse and areas indicated to be at risk of flooding. This is currently advised to the 2m+ (approx.154mAOD) from the top of the banks of the watercourse until this is verified against any further information from the Environment Agency).
- Where flow paths are identified through the development site, locate swales or other open surface water management measures in these areas to provide a clear flow path for the surface water flows which do not affect the pedestrian/ cycle and vehicular access around the site. These could also provide additional storage volume to allow for the impacts of climate change compared to current natural contours of the land where the surface water risk is currently indicated.

In the areas to the east of Barker Lane there is a low level of flood risk to the development site, with only some potential risk along the northern edge of the site near the watercourse:

- Properties should be located a safe distance from the watercourse. This is currently advised to be +2m (approx.154AOD) from the top of the banks of the watercourse until this is verified against further information from the Environment Agency.
- The ground floors of properties should be slightly elevated (e.g. approximately 150mm) above the surrounding external areas to prevent minor residual risk of flooding from the possible exceedance of local highway drains.

A detailed Flood Risk Assessment will be required to accompany planning applications. The developer will be required to design the surface water drainage system in accordance with the Flood Risk Assessment. In addition, to inform the drainage strategy for development, consultation will need to be carried out with United Utilities, the Environment Agency and Blackburn with Darwen Council as Lead Local Flood Authority.

1.7.9 UTILITIES

There are potential locations for connections to existing utilities along Yew Tree Drive. There are water main, gas, electricity, combined drainage and BT supplies located in Yew Tree Drive. Capacity calculations will be undertaken by the statutory Authorities to determine whether upgrades to the existing networks will be required to enable the site to be connected onto the network. The existing utilities within the site are illustrated on figure 19.

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.10 LANDSCAPE CONSIDERATIONS

The methodology and assessment criteria used to assess landscape sensitivity and landscape capacity assessment makes reference to the Scottish Natural Heritage and The Countryside Agency's Landscape Character Assessment (2002) and subsequent Topic Paper 6 Techniques and Criteria for Judging Capacity and Sensitivity (2006) as well as the Landscape Institute / IEMA Guidelines for Landscape and Visual Impact Assessment (2013) (GLVIA).

1.7.11 LANDSCAPE SENSITIVITY

An assessment of landscape sensitivity involves making judgements on;

- Natural factors, such as vegetation types, tree cover, and extent of semi-natural habitat.
- Landscape quality and condition
- Aesthetic factors such as scale and enclosure.

Visual sensitivity is considered separately, as part of the process of identifying key views and ascertaining the overall visibility of the site. Visually sensitivity of the landscape has been considered, with potentially visually sensitive areas due to overlooking identified within the constraints section of this document.

1.7.12 LANDSCAPE CAPACITY

Landscape capacity describes the ability of a landscape to accommodate different amounts of change. It reflects the sensitivity of the landscape itself, and the specific sensitivity to the type of development proposed.

Figure 20 identifies landscape capacity across the site. Areas labelled with the number '1' have the lowest landscape sensitivity, and therefore the highest capacity to accommodate new development without

harm. Other factors, such as topography and proximity to existing dwellings may impact on the developability of land within these areas, as identified within the constraints plans.

The diagram shows that the majority of the eastern land parcel is 'high' in terms of its capacity, except for those areas adjacent to public footpaths and within close proximity to Kay Fold Farm.

Approximately one third of the western land parcel has been identified as 'high' in terms of its capacity. This area is located within closest proximity to the existing development edge and the A6119. More sensitive areas have been identified with a lower capacity to accommodate change further north. This is due to the distinctly rural character and more elevated ground levels which potentially result in greater visibility within the surrounding context.

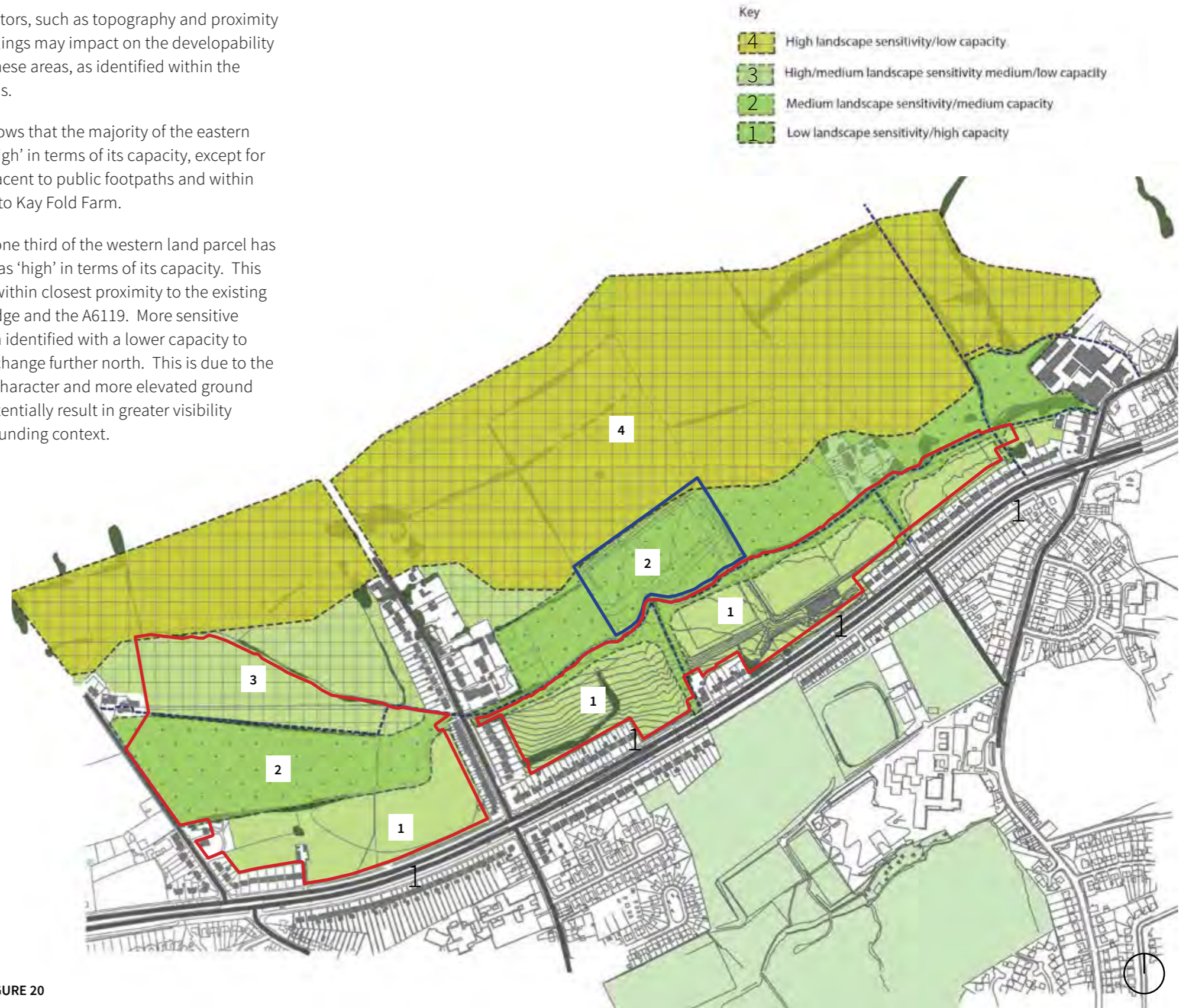


FIGURE 20
LANDSCAPE CAPACITY PLAN

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.13 LANDSCAPE CHARACTER

The site and immediate context to the north can be divided into seven clearly defined landscape character areas, through consideration of topography, field pattern, land use, condition, and boundary treatment. Figure 21 indicates the landscape areas and the key features which define each of the character areas are described below:

1. Steeply sloping open farmland

- Well defined rural character.
- Grazing land with hedgerow field boundaries where present.
- Upland farmland with steeply sloping topography.
- Visible within the local context due to elevated ground levels.
- Minimal tree cover.
- Small-medium scale field pattern.
- High quality landscape.

2. Moderately sloping open farmland

- Shares features typical of character area 1, but with more moderately sloping landform, and reduced visibility within the local context due to lower ground levels.
- Rural in character.
- High quality landscape.

3. Rolling lowland farmland

- Shares characteristics with character area 2, but less rural in character due to closer proximity to urban area, and frequent intervisibility.
- Less visually prominent in the local area due to lower ground levels.
- Some significant hedgerows and tree clusters to the east.

4. Undulating urban fringe farmland

- Distinct urban fringe character due to proximity to existing settlement.
- Ground levels rise to meet the A6119, creating a direct relationship between this key strategic route and the grazing land.

5. Steeply sloping urban fringe farm

- The character is heavily influenced by overlooking from the backs of houses along the A6119 and Barker Lane.
- Steeply sloping ground levels, which have been manipulated to accommodate developments at the edges, resulting in an urban fringe character.
- Moderate/poor quality hedgerow forms landscape feature with potential to be enhanced.
- Strong visual relationship between existing dwellings and this area.

6. Sports pitches

- Well-maintained sports pitches associated with the Rugby Club.
- Manipulated ground levels, flattened to accommodate pitch usage.
- Open character allows panoramic views from both the Rugby Club clubhouse, and from the public footpath along the northern edge of the site.
- Ground levels extensively manipulated to allow vehicular access and parking off the A6119.

7. Well contained damp grassland

- Well contained by the existing urban edge to the south and east.
- Strong visual relationship between the area and the existing urban edge.
- Direct views from the public footpath which

KEY

1. Steeply sloping open farmland.
2. Moderately sloping open farmland.
3. Rolling lowland farmland.
4. Undulating urban fringe farmland.
5. Steeply sloping urban fringe farmland.
6. Sports pitches
7. Well contained damp grassland



FIGURE 21
LANDSCAPE CHARACTER PLAN (TO BE READ IN CONJUNCTION WITH PHOTOS ON PAGE 33)

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS



KEY

- 1. Steeply sloping open farmland.
- 2. Moderately sloping open farmland.
- 3. Rolling lowland farmland.
- 4. Undulating urban fringe farmland.
- 5. Steeply sloping urban fringe farmland.
- 6. Sports pitches
- 7. Well contained damp grassland

PHOTOS ABOVE ILLUSTRATES THE LANDSCAPE CHARACTER AND SHOULD BE READ IN CONJUNCTION WITH FIGURE 21

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.14 DISTINCTIVE SITE FEATURES

The site has a number of distinctive features which are highlighted in Figure 22. These distinctive features and our response to them will drive the development of a suitable townscape for the site. The distinctive site features are described below:

- Firstly the site sits along the boundary between the urban and rural edge, this strategic position means it is influenced by and needs to respect both aspects.
- The linearity of Yew Tree Drive/ Ramsgreave Drive and the suburban housing forms the southern boundary of the site. This town character crosses Yew Tree Drive and divides the site along Barker Lane.
- A number of farmsteads sit very close to the northern boundary of the site. These are clearly rural features which influence this northern edge.
- The contours of the site are particularly steep running down north east from Yew Tree Drive. This topography will have a direct influence on the position, shape and form of the housing within these areas.
- Two features which run east-west along the northern edge of the site are a small brook and a public footpath. These form a natural boundary to the northern edge and provide a gateway to the rural landscape beyond.

The photos below and opposite illustrate examples of Rural Clusters within the locality - Changes in building orientation create a mix of frontage and gable along the road edge; a variation of building types and organic character. Clusters of buildings sit on the edge and within the wider landscape. The orientation of the buildings change to address different edge conditions and do not form a traditional block layout. Overlooking and security are provided by a mix of fronts, backs and gable ends. Amenity space is shared with a mix of hard and soft landscaped areas.



1.0 SETTING THE SCENE
1.7 SITE CONSIDERATIONS

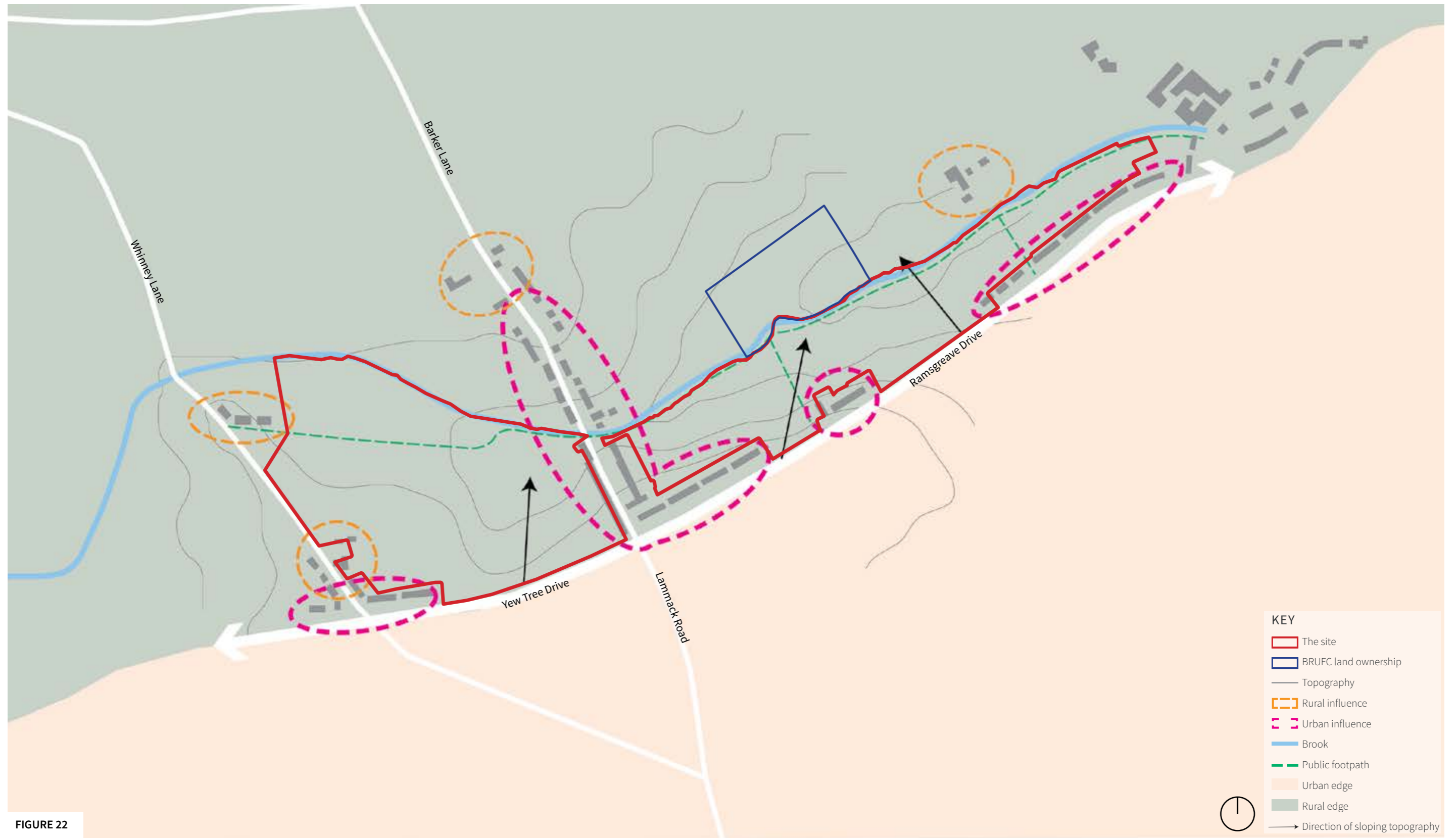


FIGURE 22

DIAGRAM TO ILLUSTRATE THE DISTINCTIVE FEATURES OF THE SITE WHICH WILL INFLUENCE THE TOWNSCAPE QUALITY AND STRUCTURE OF ANY DESIGN PROPOSALS

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

1.7.15 COMPOSITE SITE CONSIDERATIONS

The following summarises the key considerations that will need to be taken into account in the preparation of detailed scheme proposals.

The site slopes down from the southern and western boundaries to the eastern corner. The gradient is steep in areas and will have to be considered carefully in the preparation of detailed scheme proposals. In addition, a number of views along the various footpaths should be utilised to enhance legibility throughout the proposals. The highest point on the site is the southern boundary with Yew Tree Drive.

The site is an area of agricultural pasture with an historic pattern of medium sized fields, defined with remnant walls and hedgerows, which are generally of a mixed quality. These features have a landscape, heritage and/or amenity value and their retention or replacement within scheme proposals should be considered.

Watercourses run along the northern boundary and collect in the centre of the site, running out of the site on the eastern boundary. This water feature is a real site asset, providing interest and landscape amenity. Additionally the brook and adjacent areas are regarded as ecologically sensitive areas and have an important role in managing site drainage. The setting and function of these watercourses will need to be carefully considered and enhanced in scheme proposals. It is expected the brook will be a key component of the green infrastructure network/green corridor along the northern edge.

The area has a strong rural character which dissipates to a more suburban development along the southern and eastern boundaries. The northern parts of the site forms the lower slopes of the Mellor Ridge and views to and from the development will be an important consideration.

The presence of development (suburban to rural lanes and suburban frontage) is a strong local characteristic. The existing properties which back

on to the site need to be considered, with an appropriate distance between them and any new buildings.

There are some utility constraints, including an easement which runs along the eastern and southern boundary of the Yew Tree Drive site, which could be incorporated into open space.

There are several opportunities to access the site. Vehicular access will need to be tested carefully to ascertain the best options for access as development on individual parcels are brought forward. The existing network of pedestrian, cyclist and vehicle routes need to be maintained and enhanced including the provision of a combined footpath/cycleway within the green corridor. Altogether this movement network must provide safe and attractive access through the site, connect off site routes, connect to surrounding local facilities and public open space.

There is potential to integrate the land outside the masterplan/borough boundary, currently owned by the rugby club, within the development. This would need to be discussed with Ribble Valley Borough Council and meet the requirements for development in the Green Belt as set out in national planning policy.

The site considerations are illustrated within Figure 23.

1.0 SETTING THE SCENE

1.7 SITE CONSIDERATIONS

- KEY**
- ▭ Site boundary
 - ▭ BRUFC land ownership
 - ▭ Public footpaths
 - ▭ Trees & Hedgerows - of mixed quality
 - Existing trees
 - ▭ Dry stone wall
 - ▭ Water course
 - ▭ Surface water run off areas
 - ▭ Contours
 - ☀ Potential pedestrian access
 - ☀ Potential vehicle access
 - ☀ Existing vehicle access
 - ▭ Existing houses
 - ↔ Key views immediately adjacent to the site
 - ▭ Services easement
 - ▭ Important frontage
 - ▭ Underground Power Cable
 - ▭ Land Owned by BRUFC
 - ▭ Green Corridor

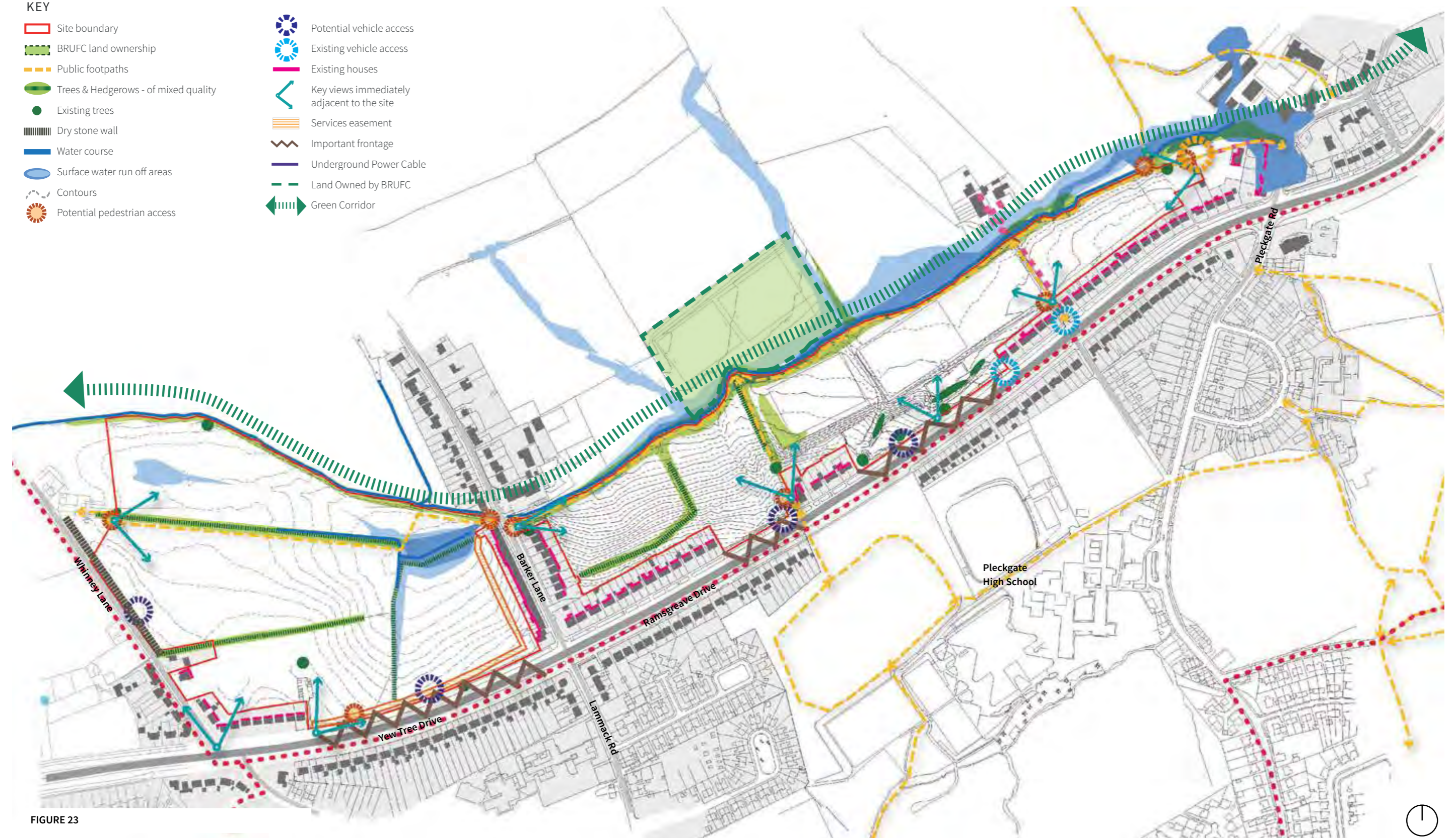


FIGURE 23
 DIAGRAM ILLUSTRATING COMPOSITE SITE CONSIDERATIONS





An aerial site plan of a campus or urban area. The plan shows building footprints, roads, and green spaces. Several red asterisks are scattered across the plan, primarily in the upper-left and central areas. Several purple circles are drawn around specific buildings or areas, notably in the lower-left and central-right areas. A large, semi-transparent blue circle is positioned in the upper-right corner of the image. The text '2.0 Masterplan Framework' is overlaid in white at the bottom.

2.0 Masterplan Framework

2.0



The following section starts with an explanation of the vision and objectives for the future development of the site. From this a set of masterplan drivers are used to introduce an illustrative masterplan. This provides the basis for understanding the guiding principles and character area guidance from which planning applications will be judged.

2.1 VISION

Our vision builds upon the unique character of the rural setting to create a high quality, distinctive and sustainable neighbourhood, with a wide choice of family housing, and will become a place where people will aspire to live.

- It will be an attractive place to live with approximately 450 new homes being provided, including a significant proportion of larger family housing and an integrated network of linked green spaces for residents to enjoy.
- The site will have a strong local identity. It will be characterised by attractive, well-designed buildings and spaces and will comprise of a number of distinctive areas with their own unique character which responds to the characteristics of that particular part of the site.
- Development will maximise existing views from the site and will respond positively to the topographical character of the site.
- Existing landscape features will be enhanced, informing the landscape framework and network of green spaces, the landscape framework will provide a dual function with a network of safe and attractive links to the adjacent urban area.
- The site will be well-connected to existing facilities and services, with a permeable layout that maximises linkages and integration within the site and to the wider area. The existing footpath network within and adjacent to the site will be enhanced to encourage walking and cycling with specific links to the Weavers Wheel cycle network and improved access to public transport services as an alternative to travelling by car.
- A 'green corridor' will provide opportunities for informal recreation and childrens play. The character will contribute to the area's green, leafy character and provide a transition to the open countryside as well as further opportunities beyond the site.



2.2 OBJECTIVES

The development objectives are to:

- Provide a **balanced mix of high quality housing**, including family housing, with a mixture of housing typologies;
- Provide **sustainable energy efficient** housing;
- Create a **high quality urban extension** to Blackburn which is well-connected visually and physically to the existing urban area and surrounding countryside;
- Create a **varied and distinctive environment** in new housing, landscape and public realm through local design character and cues from the rural character;
- Use the topography of the site to **inform the design response**;
- Create a **well-connected** and legible network of **streets**;
- Create a **well-connected** and legible network of **pedestrian and cycle movement** corridors within the site and linking the site with facilities outside;
- Protect and enhance the watercourses and any sensitive habitats on the site to **increase biodiversity**;
- Sustainably **manage surface water run-off**;
- Create an **attractive landscape corridor** along the northern boundary adjacent to the watercourse to provide a robust and enduring new Green Belt boundary with integrated SuDs, habitat enhancements and walking/cycling routes;
- To provide a **network of green spaces** within the development to break up the built form;
- Create **distinctive character areas** across the site taking references from the vernacular of the wider area. The new built form should aim to form an attractive transition between the rural and urban edges taking account of views into the site.

ABOVE: ARTISTS IMPRESSION OF A NEW SEMI-RURAL URBAN EDGE

2.0 MASTERPLAN FRAMEWORK

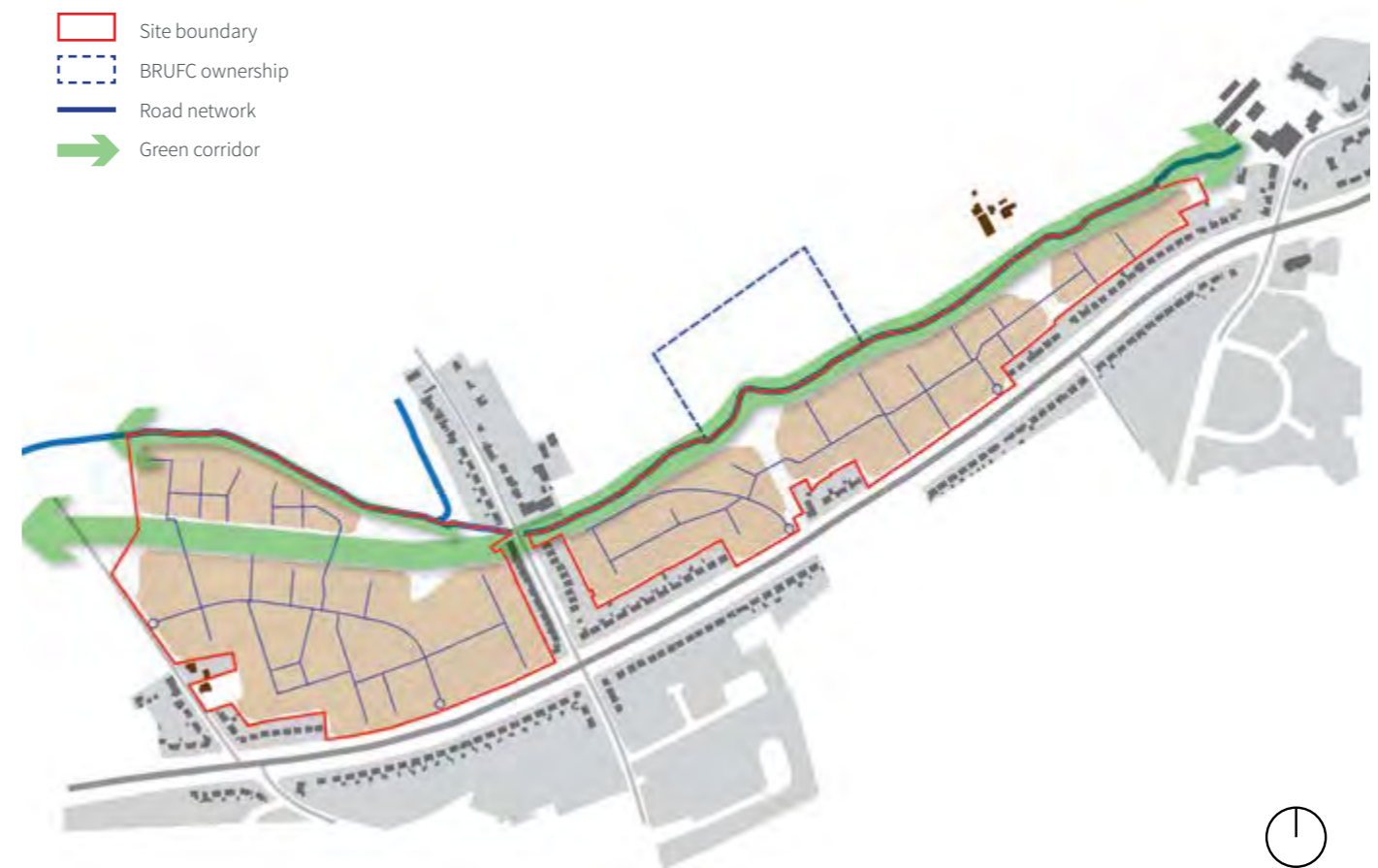
2.3 MASTERPLAN DRIVERS

The following overarching design principles have emerged through the site analysis and technical studies work undertaken and form the basis of our strategic approach to the shape and form of any future development of the site. The principles should be considered when formulating detailed design proposals for the site.



Green Corridor and Rural Edge

Provide an attractive landscape corridor along the northern boundary adjacent to the watercourse to provide new green belt boundary with integrated SUDs, habitats, and walking/cycling routes. This feature helps to blend the transition between urban and rural and integrate the development with Blackburn and the wider countryside. Additional north south corridors would be created that correspond to clear desire lines and landscape features to create an attractive network of connected spaces.

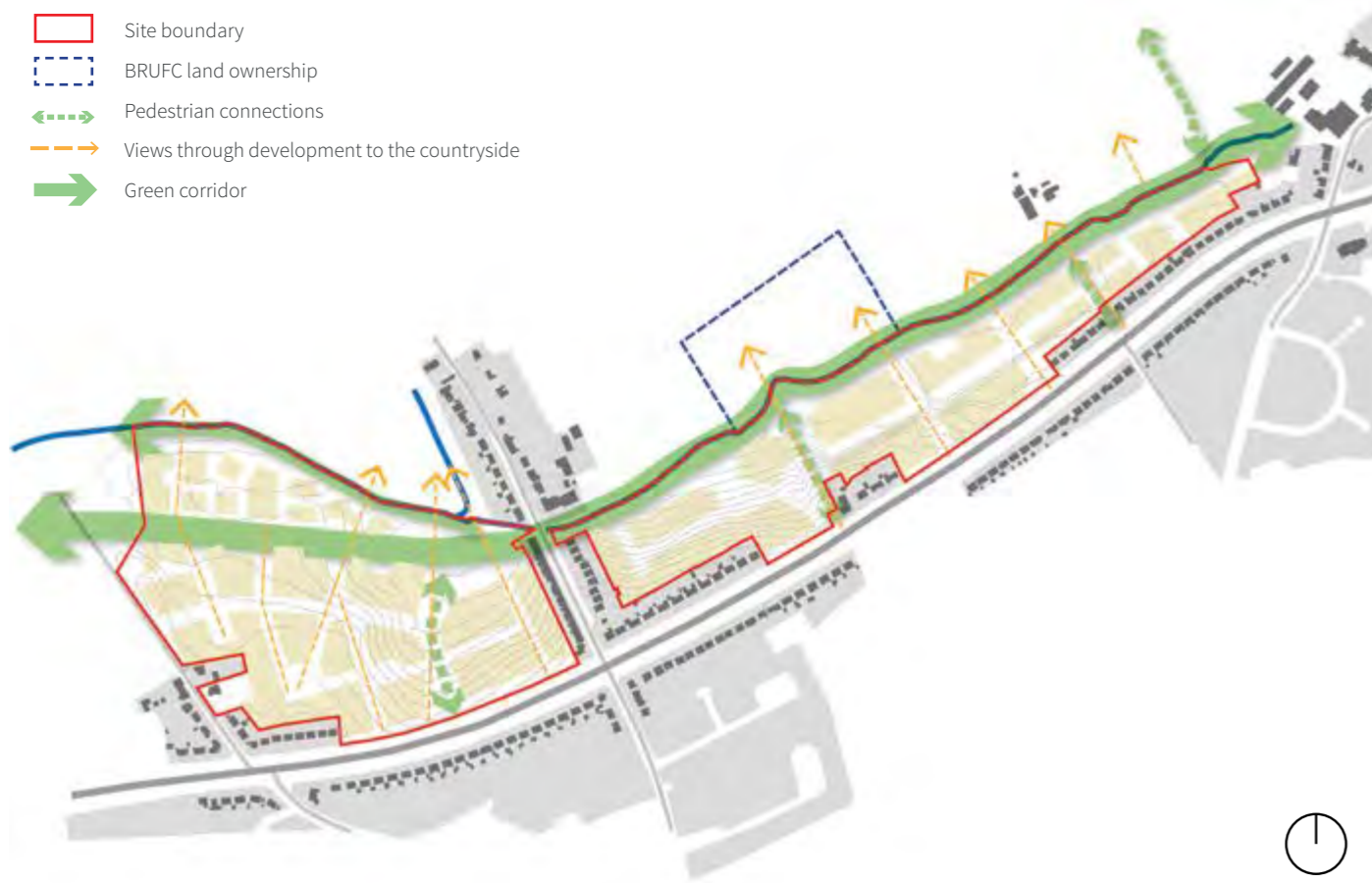


Layout Structure

The topography of the site will influence how the site is accessed and the road structure should facilitate the requirement to create a permeable network of north-south routes along with a focus along east-west lanes.

2.0 MASTERPLAN FRAMEWORK

2.3 MASTERPLAN DRIVERS



Views and Connections

The development form should be broken up with strong north-south visual and physical linkages which connect the existing and proposed neighbourhoods with the existing footpath network and surrounding open countryside.

By drawing the connections through the neighbourhood new housing will feel connected to the surrounding open countryside adding value and a sense of place.

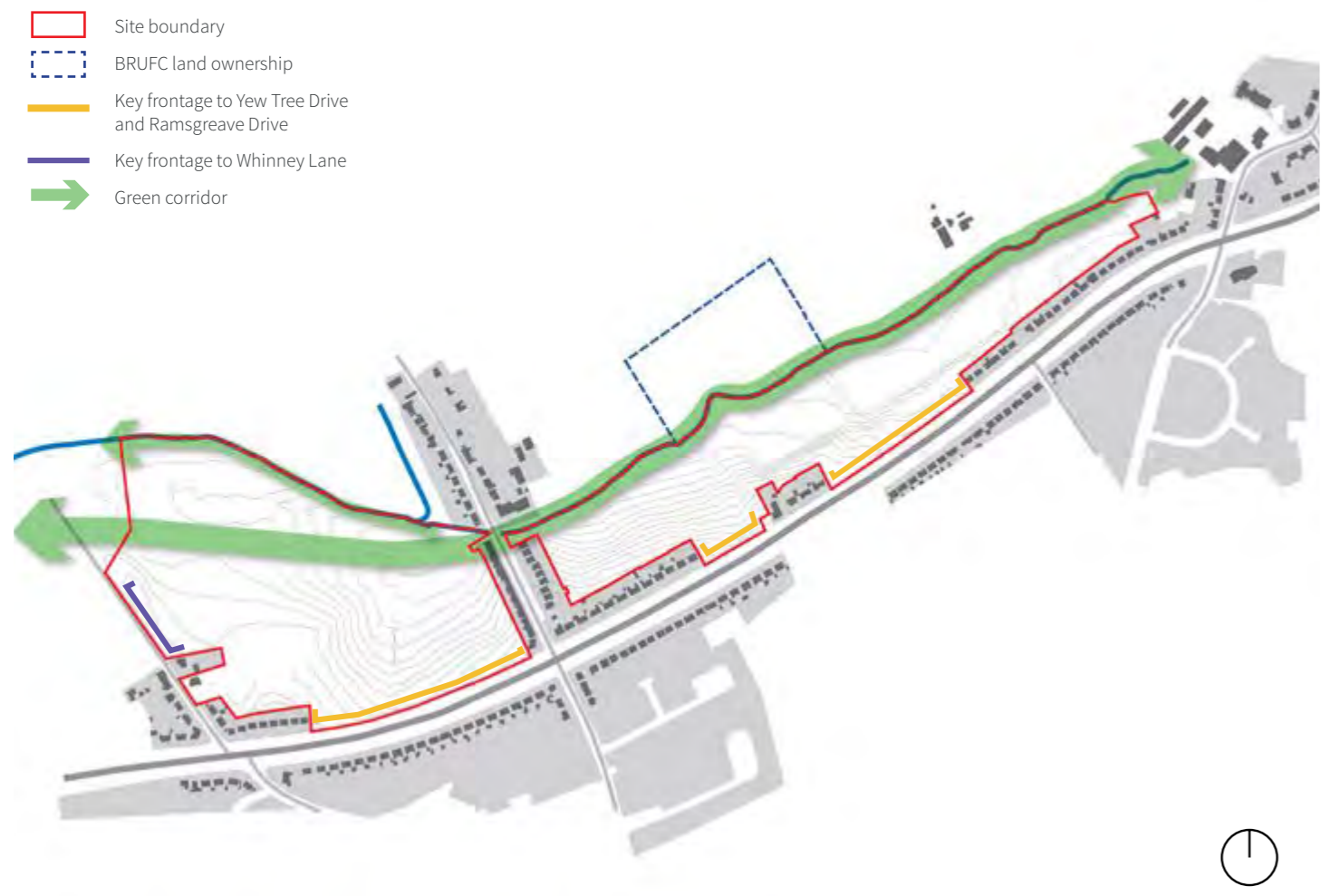


Development and Character

The topography and strategic position between urban and rural will directly influence the capacity and character of particular parts of the site. Four distinct characters are suggested, some being more urban in character, whilst others taking their cues from the rural characteristics to the north.

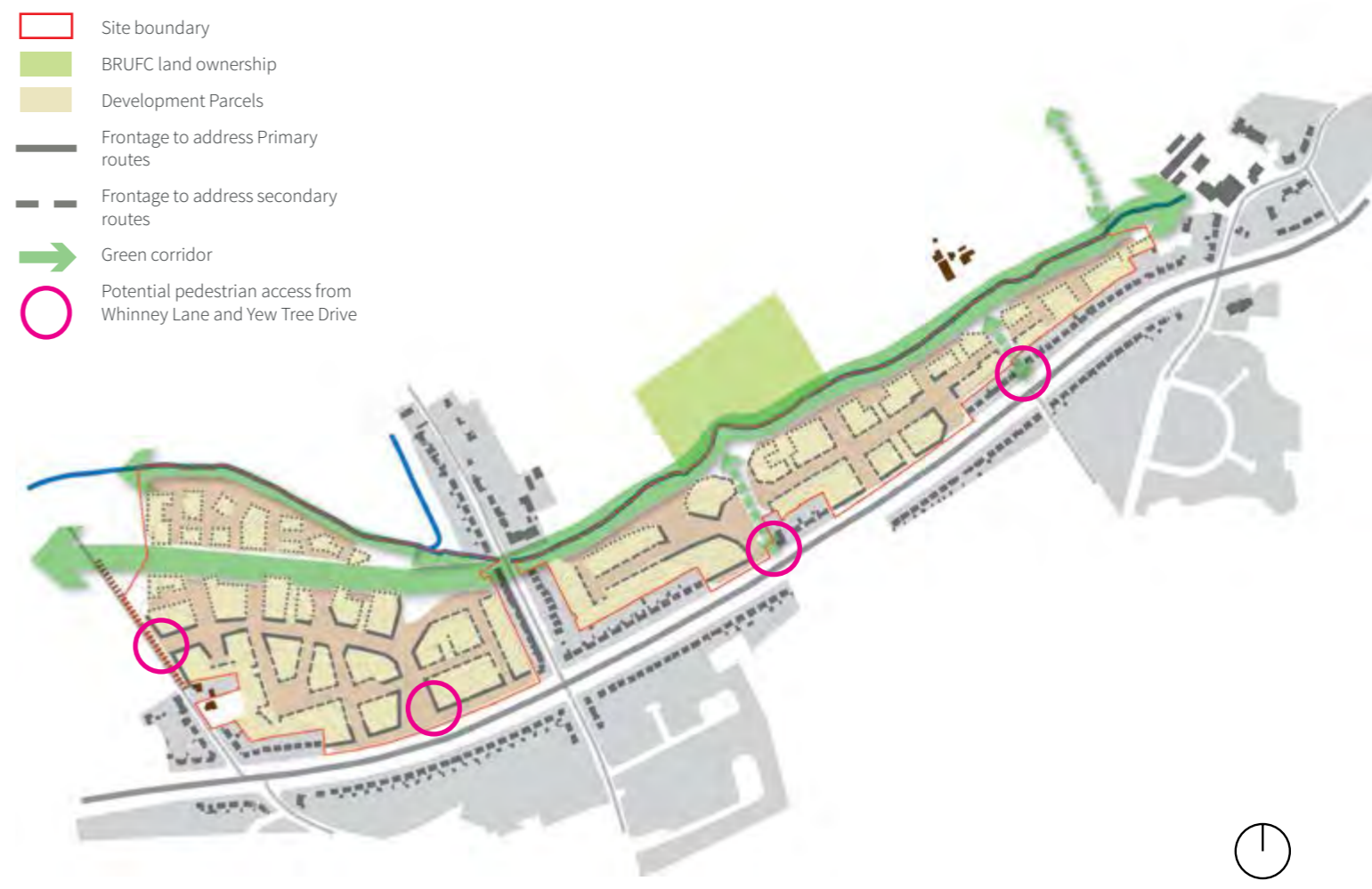
2.0 MASTERPLAN FRAMEWORK

2.3 MASTERPLAN DRIVERS



Frontage & Interface

It is important that the development has a strong physical and visual relationship with Yew Tree Drive and Ramsgreave Drive, to reinforce the principle that this development will be an addition to the wider Blackburn townscape and community.



Creating Place

The overall neighbourhood structure should be permeated with gateways, spaces, squares and courtyards in order to provide variety and interest, improving legibility. These interfaces should occur at natural nodal points within the site. Grain and density can increase at these junctures in order to create continuity and enclosure. However building form could become looser around the rural open countryside and Green Corridor.

2.0 MASTERPLAN FRAMEWORK

2.4 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK



FIGURE 24:

ILLUSTRATIVE MASTERPLAN FRAMEWORK

2.0 MASTERPLAN FRAMEWORK

2.4 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK

2.4.1 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK

The masterplan framework sets out a spatial representation of how the area will be developed and identifies in the following sections how the character areas, streets, green spaces and transport corridors could be arranged in order to achieve the vision for the area as a high quality sustainable urban neighbourhood.

The illustrative masterplan framework (Figure 24) demonstrates the key principles for the layout of the site and includes:

- A number of attractive primary routes accessing the land parcels from Yew Tree Drive, Ramsgreave Drive and Whinney Lane;
- Network green spaces and corridors to form focal spaces and enhance views for the enjoyment of residents;
- An enhanced landscape corridor along the brook to the north;
- An interconnected network of streets;
- Improved footpath/cycle connections with links to the Weavers Wheel cycle network;
- The development of 4 distinct character areas;
- Housing fronting streets and spaces set out in perimeter blocks with a varied range of plot sizes and building footprints;
- Density of housing to vary across the site with less dense housing adjacent to the countryside area.
- Built form to respond to rural characteristics of the countryside setting of the Mellor Ridge.

2.4.2 GUIDING PRINCIPLES

In order for the land to the north of Ramsgreave Drive to become a high quality, sustainable neighbourhood the Council will seek high quality designs for housing, in accordance with the design guidance specified within this document. Guidance on housing requirements in relation to density and mix is set out below.

Housing Layout

Housing delivered on the site will be expected to be well-designed, good quality homes which provide variety in character.

Of particular importance is the requirement to create a semi-rural character away from Ramsgreave Drive and Yew Tree Drive, and to create a positive relationship with the open countryside and open spaces within the scheme.

Any development proposals should respect the rural landscape and be arranged in perimeter blocks of varying character.

The development will be laid out in a series of small to medium development blocks to give a fine grain layout which support a connected network of streets and spaces.

Blocks should be designed as perimeter blocks where buildings front the street or public space and there is a clear distinction between public and private realm. This will ensure each dwelling has a private rear garden and will provide eyes on the street creating a safer and more secure environment.

The appropriate use of variety in building lines is however encouraged in order to create visual interest

Block size should be considered alongside the need to maximise the permeability of the development,

particularly along a north south axis, and encourage walking and cycling while providing for the effective layout of homes and the efficient use of land. Incursions may be made into the block, for example to accommodate car parking.

Layouts should seek opportunities when terminating lanes or driveways to create a series of attractive places which are not defined by highway geometry.

Development should be in accordance with the Council's separation distances and seek to give more generous privacy distances to existing dwellings.

Gateways

A number of entrances are proposed into the site from the surrounding roads and development will be required to create attractive and distinctive gateways.

This can be achieved by building orientation, architectural expression or landscape features. There are important gateways from Ramsgreave Drive, Yew Tree Drive and Whinney Lane which should be emphasised using visually attractive building forms or landscape features. Gateways from Ramsgreave Drive should explore options for creating attractive corner buildings, boundary treatments and /or where this is feasible utilise landscape features to give a recognisable gateway. The rural character of Whinney Lane and existing stone boundary wall should be retained at the access gateway, building frontage and orientation can be used here to give a village character.

Landmarks

Local landmarks can comprise key feature buildings, spaces, mature trees. These should be used to help create a sense of place and identity, helping people orientate themselves around the development.

Landmarks and feature buildings can be used to emphasise key vistas, terminate a view or provide a focus to an area.

Landmarks can exist within and beyond the masterplan area such as the Mellor Ridge line or farm building. Buildings can be orientated and designed to accentuate a corner for example, or elevated properties can incorporate architectural features to exploit panoramic views.

Focal points

There is also scope to incorporate a range of reference points from within and outside the site to create a distinct sense of place, such as some isolated trees or hedgerows. Incidental informal spaces have been shown on the masterplan to break up the built form, open up view or accentuate a landscape feature.

Key nodes and corners should be identified and emphasised using high quality public space, or other interesting built form features. The landscape elements will also form the key structural elements that can form important focal elements.

Important Views

There are views from outside the site though these are limited but can be glimpsed from footpaths and the adjacent countryside.

There is also an opportunity to provide a range of formal and informal views from within the site including vistas created by green corridors as well as more glimpsed and unexpected views.

The design of the development should take advantage of land form to maximise views, or frame views out across the landscape.

Further guidance on the layout of development in different parts of the site is provided in the Character Area section of this document.

The development will be required to provide open space and landscaped areas as described in the green infrastructure framework which will also incorporate

2.0 MASTERPLAN FRAMEWORK

2.4 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK

sustainable drainage infrastructure.

Open spaces should be overlooked by properties to ensure surveillance and safer places. The green spaces should be multifunctional providing an ecological, amenity and recreational resource. The layout of development parcels should be informed by the existing and new landscape, such as hedgerows, SuDS/ swales, stone boundary walls and tree groupings.

Housing Mix

The Council's ambition, supported by Local Plan policies, is that housing delivery on this site offers a wider choice of housing with regard to house type, quality and size, and is attractive to new residents looking for higher value market housing, principally larger family homes. With this in mind the Council expects the overall development to provide a balanced mix of high quality housing including the provision of a good standard of living space, predominantly 3, 4 and some 5 bed family homes, with a mixture of housing typologies.

The Council's affordable housing policy will apply; refer to Section 1.2 Planning context.

Housing Density

Housing delivered on the site will be expected to contribute towards the creation of a green leafy neighbourhood with generous private gardens to dwellings.

Densities should vary throughout the site with lowest densities toward the countryside edge and highest closest to existing urban area.

Whilst high densities are appropriate in parts of the Borough, the land to the north of Yew Tree Drive and Ramsgreave Drive is situated on the edge of the urban area and will provide the transition between the built-up area to the south east and open countryside to

the north west. Accordingly, although there is a need to ensure that land is used efficiently and that the development will deliver reasonable housing numbers to meet identified needs, the density of development must also reflect the character of the surrounding area and respond to the setting of the site, together with the aspiration to create a neighbourhood with a semi-rural character.

The density diagram (Figure 25) indicates how the density of development could be provided throughout the site. Further guidance on the density of development in different parts of the site is provided in the Character Area section of this document.

The semi-rural characteristics can be defined by a variety of densities, notwithstanding this a greater amount of landscape and space will be required around the open countryside to mitigate against visual impacts from visual receptors (as described earlier in section 1.5, page 17 of this document) using the public footpath.

It is expected that densities will however vary across the site. Areas of medium density (25-35 dwellings per hectare) will be expected towards existing urban areas along Yew Tree Drive and Ramsgreave Drive.

Densities will decrease towards the middle of the site (c 20- 30 dwellings per hectare) to provide an appropriate transition to the outer edge of the development.

Densities around the interface with the open countryside will reduce to c 15-25 dwellings per hectare in order to reflect the semi-rural character and allow for appropriate mitigation.

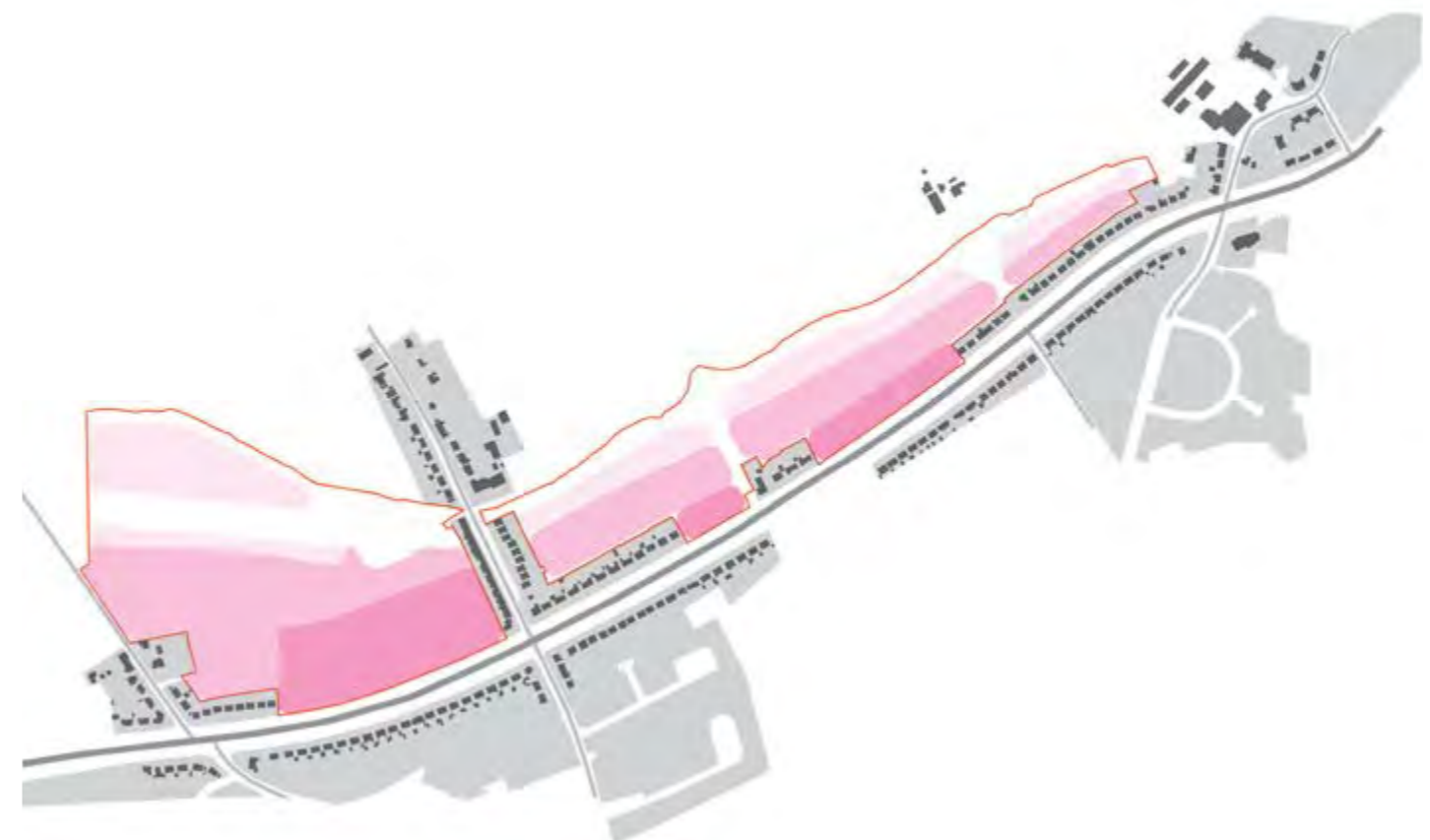


FIGURE 25:
DIAGRAM ILLUSTRATING DENSITY

KEY

- Site Boundary
- Medium/high: c25-35 dph
- Medium/low: c20-30 dph
- Low: c15-25 dph



2.0 MASTERPLAN FRAMEWORK

2.4 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK

2.4.3 Movement Framework

A key requirement of the movement framework is to provide site wide connectivity both through and between the individual land parcels and to surrounding services and facilities. The site is bounded to the south by Yew Tree Drive/Ramsgreave Drive corridor which is a dual carriageway and bypass providing access to the wider route network.

In order to secure a connected sustainable layout the movement framework for the site will require a number of access points from the dual carriageway and an access from Whinney Lane to link to an internal network of secondary streets and lanes.

In order to create a residential character along this road corridor it will be necessary to implement a highway improvement scheme to reduce vehicle speeds and includes new crossings/junctions to provide safe and accessible access to the site and the wider road and footpath/cycle network.

One of the main aims for all new development on this site is to encourage sustainable modes of transport and minimise use of the private car. This section sets out the requirements for the site and identifies key principles for the character of the streets and networks (a summary of these elements is identified within Figure 26).

Key Guiding Principles:

- An accessible and connected neighbourhood with attractive roads and footpaths.
- A number of new vehicular access points off Ramsgreave Drive, Yew Tree Drive and Whinney Lane will ensure the development is conveniently accessed and well connected.
- Enhanced and improved footpaths across the site to link to surrounding neighbourhood centres, schools and green spaces within the site.

- Accessible and safe links to Weavers Wheel cycle network.
- To encourage active travel with a provision of an 3m wide combined footpath/cycle path along the brook within the new green corridor to the northern part of the site.
- The provision of a series, shared streets and lanes that respect rural character.
- A hierarchy of streets provided which vary in pedestrian vehicle balance and include shared pedestrian/vehicle streets and spaces as indicated.
- A highly permeable network of north south streets to provide connectivity between open countryside and the new neighbourhood.
- Environmental improvements will be required to Ramsgreave Drive, Yew Tree Drive and Whinney Lane to help slow vehicle speeds and give the road a more residential characteristic.
- Retain rural character of Whinney Lane.

Network Layout

The network layout should provide a hierarchy of routes to create a connected and permeable layout and avoid significant impact on the highway network established. It is expected to include a number of primary routes (depending on phasing of the development) accessed from the dual carriageway, a smaller single access from Whinney Lane, and a series of secondary routes and /or lanes. The route network will be required to provide attractive streets and lanes supplemented with planting and landscape features to reflect the urban rural transition. Where possible shared surfaces will be encouraged and maximum speeds to be no more than 20mph.

The route network will be orientated north-south in the main, to exploit views through the site to the countryside beyond with a small number of east west routes that could provide links across land parcels and

adjacent areas.

Primary Street Design Principles

The Primary routes are intended to form the main vehicle links and connect to the more intimate secondary streets and lanes. They are intended to have a relatively intimate feel and be tree lined with formal landscape planting. Where possible building enclosure and variation in width should be integrated into the design to reduce vehicle speeds and avoid rat running. Pedestrian footpaths and/or cycle paths to line at least one side of the carriageway.

The masterplan shows a primary route to the western portion of the development which is expected to have a village character to reflect the rural character of this part of the masterplan.

Secondary Street/Lanes Design Principles

To have an intimate informal character fronted by housing and predominantly to be shared surfaces. Pedestrian footways either to one side of the carriageway or integrated within the carriageway. Informal planting to be incorporated in the street scene but front gardens behind hedgerows or low walls are expected to provide the main greening elements. Permeable resin or paving to be preferred treatment of the carriageway surface.

Bus Access

The Council is very keen to maximise the use of sustainable travel and would therefore like to explore opportunities for the development to contribute towards the enhancement of the existing bus service.

Although it is presently envisaged that bus services will not be directed through the site, bus routes can change over time and the primary routes should therefore be designed to ensure that it is physically capable of accommodating a bus route in the future.

Pedestrian and Cycle Links

Development proposals will be expected to provide an integrated network of safe footpaths and cycle routes which provide access through the site and connect into the wider off-site routes.

Development should also provide:-

- Enhanced footpath/cycle way along the green corridor adjacent to the brook.
- Safe access to the Weavers Wheel cycle network at junctions and crossings.
- A permeable layout across the whole site.
- A pedestrian/cycle network that provides convenient access to local facilities.

The footpath and cycle network should integrate with the landscape structuring elements/green infrastructure network across the site and, where appropriate, streets should be designed to provide priority to cyclists and pedestrians.

Travel Plans

The Council is keen to maximise opportunities for sustainable transport that encourage alternative modes of transport to travelling by car such as walking, cycling, public transport and car sharing. Development proposals should be accompanied by a Travel Plan which identifies specific, deliverable measures to encourage the use of sustainable forms of transport.

This co-ordinated plan should establish a package of measures that will be implemented and monitored to mitigate the impacts of the proposed development and to promote means of transport that are beneficial to health. The plan should help to support sustainable economic growth, promote social inclusion and assist in reducing the impacts of climate change.

2.0 MASTERPLAN FRAMEWORK

2.4 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK

Developers may also be required to submit a Construction Travel Plan for the agreement of the Council, prior to the commencement on site. This should demonstrate how the impact of construction traffic will be kept to a minimum during all phases of the development. It should include the proposed access arrangements to the site for construction traffic from the surrounding highway network.

Parking

Vehicle parking across the development and provision for cycle storage should be in accordance with the Council's adopted standards unless alternatives can be justified.

Car parking must be carefully planned and integrated sensitively into the development to ensure it does not visually dominate street scenes or provide obstructions to pedestrians and cyclists. The proposed parking arrangements should support the creation of safe and attractive streets.

The Council's preference is for a range of parking solutions to be applied. They should be a combination of on-plot, off-plot and on-street solutions. An acceptable form of car parking will include:

- Predominance of on-plot parking and some on street visitor parking;
- On-street parking either as marked or unmarked bays adjacent to the carriageway;
- Front forecourt parking to be set within high quality planting to avoid dominance of the car in the street scene;
- Well-designed courtyard parking within perimeter blocks or mews streets;

- In curtilage parking to the front or side of the property, and;
- Garage parking ideally with an additional space in front, between dwellings. Detached garages on primary and secondary routes should normally be positioned behind the principle elevation of the dwelling it serves or integrated within the built form in order to minimise the impact on the street scene.

In all instances the car parking solution should enable parking close to the dwellings which they serve and should be convenient to use in order to discourage inappropriate on-street parking.

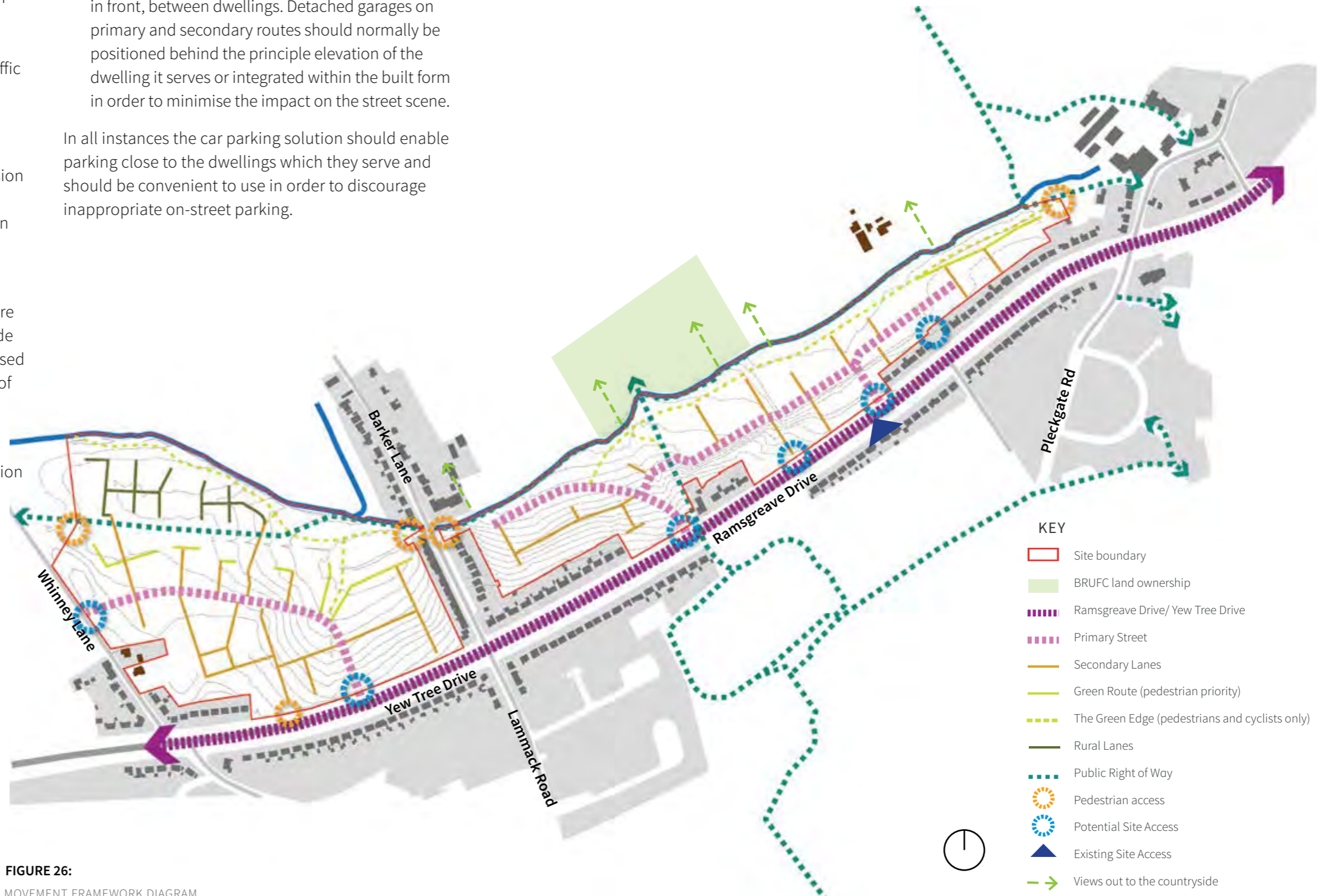


FIGURE 26:
MOVEMENT FRAMEWORK DIAGRAM

2.0 MASTERPLAN FRAMEWORK

2.4 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK

2.4.4 Green Infrastructure

An existing brook forms the northern boundary to the site, presenting an opportunity to establish a green corridor that enhances the watercourse and existing habitats as well as incorporating above ground sustainable urban drainage solutions. Additional landscape features and new green spaces will create a distinctive environment that respects the landscape assets of the site.

The Council's Open Space Assessment concludes that the site is deficient in access to a number of open space typologies including parks and gardens, children's play space and amenity/grassland. Acknowledging that planning policy requires these deficiencies to be addressed the Council's preference is for this to be provided on site as an integral part of the green infrastructure framework.

It is recognised that there is a good supply of formal facilities within easy access of the site and therefore a contribution to formal play provision off-site is not required. An outline of the green infrastructure framework is shown within Figure 27.

Key Guiding Principles:

- Attractive green corridor to be created along entire length of northern boundary alongside the brook. This corridor should be multifunctional including creation of robust Green Belt boundary, provision of amenity/play spaces and ease of connectivity by pedestrians and cyclists through the site and to surrounding area and facilities.
- Sustainable urban drainage systems to be incorporated within green infrastructure.
- Green infrastructure network should enhance connectivity between green spaces on site.
- Planting should include native plant mix to reflect local vegetation communities and encourage and enhance biodiversity.
- Variety of green and open space typologies to be provided on site to break up the built form, to form focal points and open out views and provide areas of play and enjoyment.
- Tree lined streets to form an important element of the development/green infrastructure framework.
- Informal children's play areas should incorporate natural structures and encourage activities that reflect the interests and character of the local area including, for example, natural play and walking trails.
- Design, layout and specification of the green infrastructure should provide a durable, high quality solution that offers permanence and ease of maintenance by the developer/landowner.

Green Corridor

The green corridor along the northern edge of the housing allocation site is required to be attractive, multifunctional, to be responsive to the established landscape characteristics and local biodiversity (as described in section 1.7 of this document) and to contribute to the creation and establishment of a new and permanent boundary to the Green Belt.

In addition to the above, the green corridor will be an important transition between the development and the surrounding countryside.

A number of uses/activities that will need to be provided have all been identified and include the following. Please note this is not an exhaustive list; the Council would welcome additional imaginative uses/activities that will enhance the amenity of local residents, biodiversity, connectivity and the setting of the development.

Uses/activities identified include:

1. New Green Belt boundary
2. Play
3. Combined footpath/cycle route
4. Sustainable urban drainage (SuDs)

1. New Green Belt Boundary

The North Blackburn Development Site is a small scale urban expansion into land previously designated as Green Belt along the northern boundary of the borough adjoining the Ribble Valley. The land was formally released from the Green Belt and allocated for housing with the adoption of the Local Plan Part 2. This change of boundary has positioned the edge of the Green Belt along the shared administrative boundary between BwD and Ribble Valley Borough Councils. In accordance with national planning policy there is a requirement when establishing a new/amended Green Belt boundary to have regard to the purposes of the Green Belt and their intended permanence in the long term. It is expected that the boundary is clearly defined using physical features that are readily recognisable and likely to be permanent. The existing stream along the northern margin of the site is a suitable feature to use as the basis for the new boundary. However it is expected that robust and complementary landscape work, including planting will be provided along the northern edge to reinforce the definition of the Green Belt and the green corridor.

2.0 MASTERPLAN FRAMEWORK

2.4 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK

KEY

- ▭ Site boundary
- Existing Water course
- ▭ Green corridor to provide amenity space, combined pedestrian/cycle connections and robust Green Belt boundary
- ▭ BRUFC land ownership
- ▭ Potential location for SUD's
- - - Potential green connections from Ramsgreave Drive to the Ribble Valley to the north
- Potential locations for green spaces, parks and play areas
- ▬▬▬ Existing hedges to be retained and integrated within the development
- - - Contours

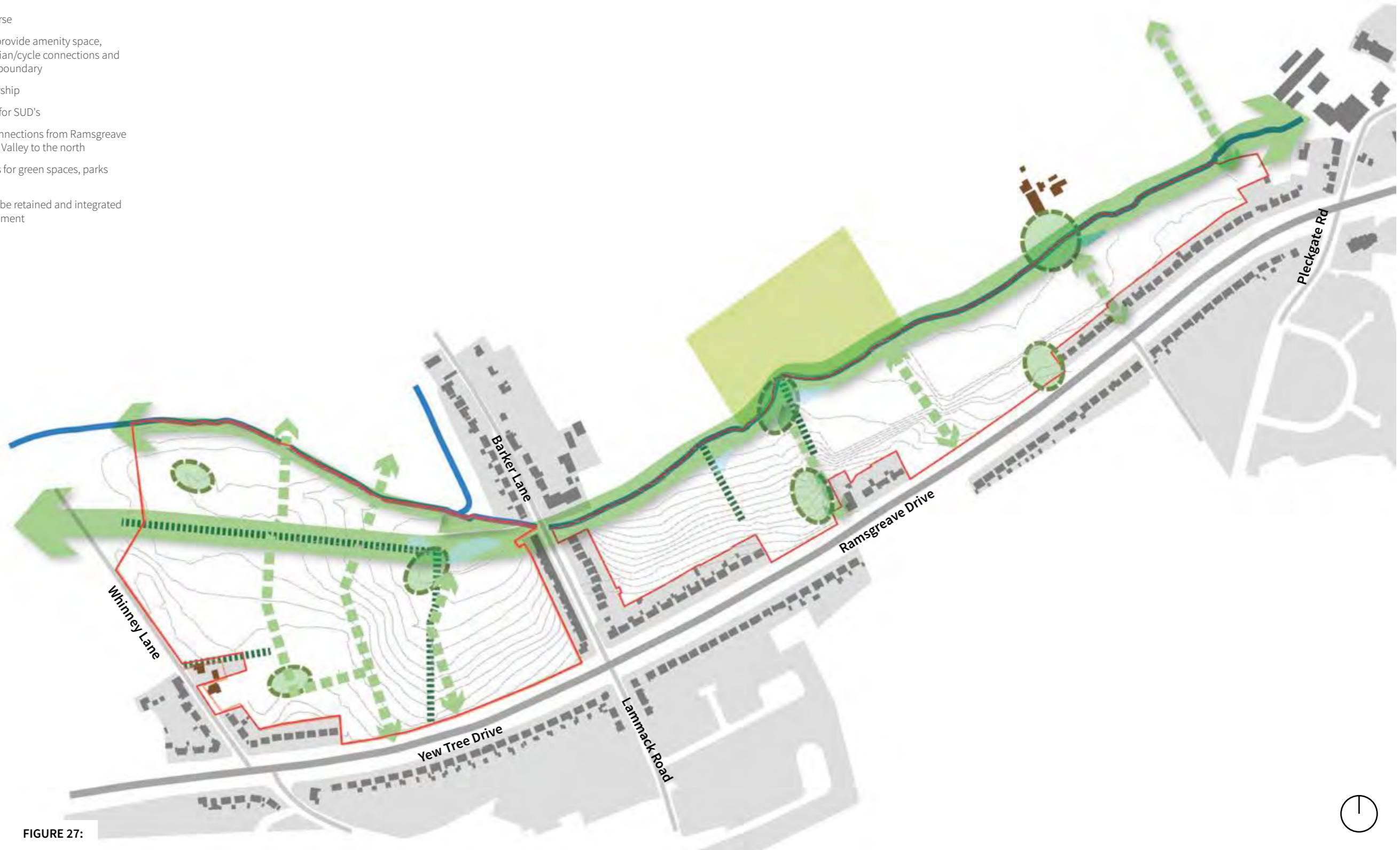


FIGURE 27:
GREEN INFRASTRUCTURE



2.0 MASTERPLAN FRAMEWORK

2.4 THE ILLUSTRATIVE MASTERPLAN FRAMEWORK

2. Play

The green infrastructure network offers a great opportunity to integrate and link informal play through the development.

Some of the guiding principles for these play features include:

- A variety of play features/play experience should be provided to encourage exploration and imagination.
- Play elements should reflect the history of the site and the local context. This will encourage the creation/ awareness of site identity and local distinctiveness.
- Opportunities should be made to integrate play into the landscape to encourage informal play and a connection to the natural environment.
- Play spaces should be well connected by pedestrian friendly footpath routes to create an integral network of play facilities.
- Play facilities should be actively overlooked by housing to contribute to the creation of a safe environment.

The following key guiding principle for green infrastructure will apply equally for play areas:

- Design, layout and specification of the green infrastructure should provide a durable, high quality solution that offers permanence and ease of maintenance by the developer/landowner.

3. Combined Footpath/Cycleway

Reference has been made in various sections of this document to the importance of connectivity across the development and to the surrounding area. A key structuring element of the green corridor is the integrated footpath and cycleway. Scheme

requirements for this have been set out in the 'Movement Framework' (section 2.4, page 48).

4. Sustainable Urban Drainage (SuDs)

The developer will be required to ensure that sufficient greenspace is provided for an above ground SuDs scheme that complies with the current national standards for Sustainable Drainage Systems.

The preliminary assessment of surface water storage requirements including estimated storage volumes and areas (m2) was referenced in Section 1.7.3 Site considerations – Flood risk constraints and drainage considerations.

The illustrative masterplan has taken these requirements into account and has provided sufficient greenspace roadside and along the northern corridor, on both the west and east land parcels of the development site to provide above ground water attenuation storage in the form of SuD's. This excludes the 8m buffer from the watercourse generally required by the Environment Agency for access for maintenance.

Developers will need to ensure that sufficient space is allowed for SuDs features as an integral part of individual scheme detailing. Due consideration must be given also to operation and maintenance requirements.

Developers are additionally encouraged to consider complementary measures for reducing surface water runoff including the use of permeable surface materials, encouraging the use of water butts, incorporating water harvesting for domestic uses (e.g. toilet flushing) and delivering roof water to planters.



ABOVE: PHOTOGRAPHS OF EXAMPLES OF PLAY SPACE AND INFORMAL AMENITY SPACM

2.0 MASTERPLAN FRAMEWORK

2.5 CHARACTER AREA GUIDANCE

2.5.1 IDENTIFYING CHARACTER AREAS

The development naturally sub-divides itself into two parcels east and west of Barker Lane, within these two parcels 4 distinct character areas have been identified that will have their own characteristics (Figure 28).

The character areas have been defined by landscape spaces, scale, density and built form and provide appropriate variety and richness contributing to a sense of place.

Key Guiding Principles Considered:

- Creation of 4 distinct character areas which complement features of the site and housing in adjacent streets. This could include materials, density and views into and from the site.
- Landscape elements to form a key aspect of character with tree-lined streets and shared surfaces taking references from the rural character.
- Dwellings to front a connected network of streets, lanes, and green corridors.
- Provision of a variety of plots and house types. Possibility to incorporate mews and courtyard developments.
- Creation of series of attractive public spaces that relate to landscape features within the site.
- Utilise level changes to provide interest across the site and make the most of views.

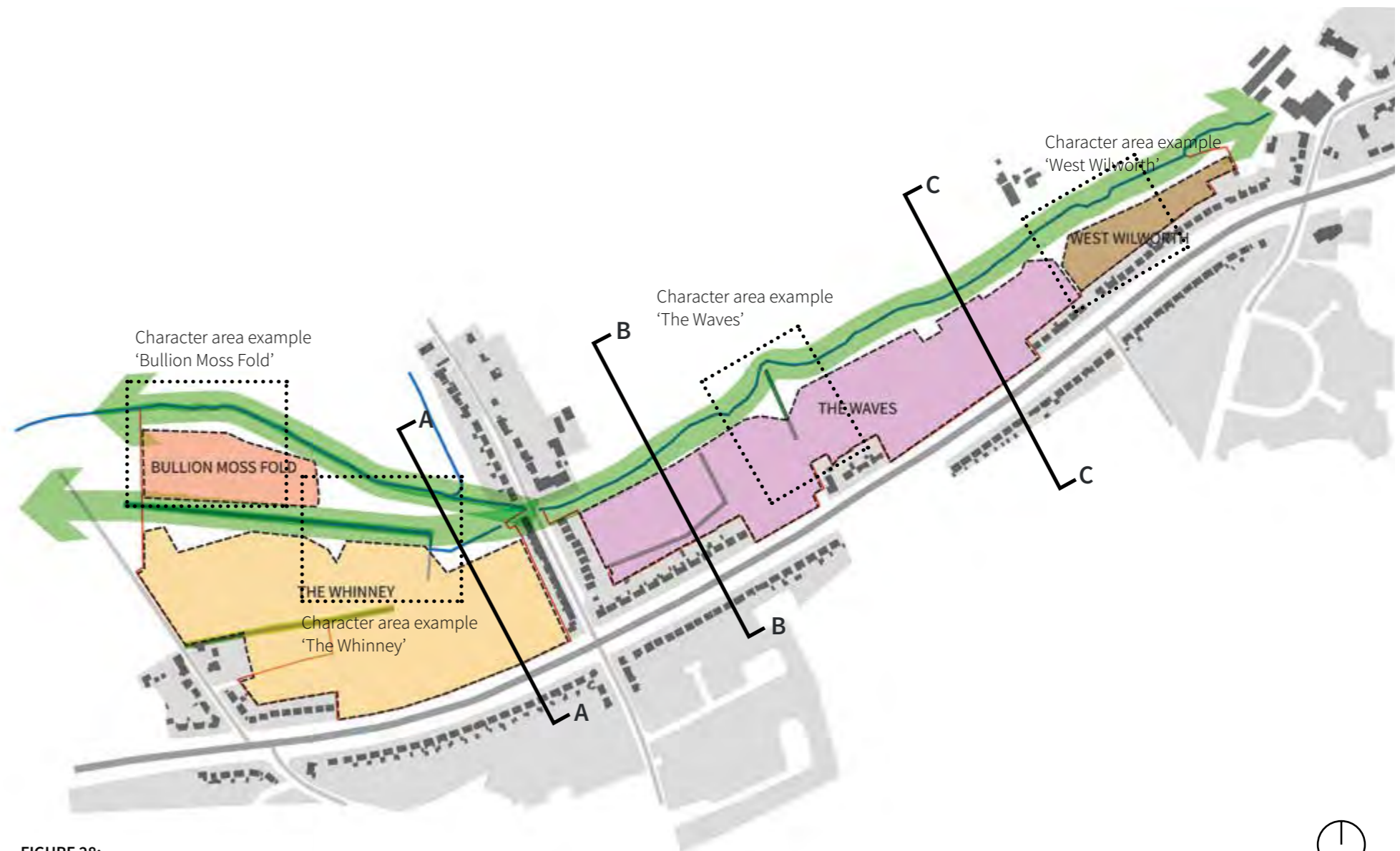


FIGURE 28:
DIAGRAM ILLUSTRATING CHARACTER AREAS

The Whinney

A mix of predominantly brick and stone housing, transitioning from a more formal, urban character along Ramsgreave Drive to a softer, informal feel along the landscaped corridor.

The Waves

A mix of brick and rendered properties of varying typologies with a more formal character influenced by green streets and corridors.

Bullion Moss fold

Organic in character with a rural feel. A mix of brick and stone; detached, semi-detached and terraced cottages with internal courtyard spaces and mews streets, making reference to rural courtyards.

West Wilworth

Informal in nature, with large detached and semi-detached properties arranged around shared courtyard spaces and the landscape corridor. Materials to be redbrick with slate roofs, scope for stone fronted dwellings facing green corridor.



2.0 MASTERPLAN FRAMEWORK

2.5 CHARACTER AREA GUIDANCE

2.5.2 THE WHINNEY

Buildings should be a mix of predominantly brick and stone but will still retain an informal character. The village lane will form centre piece of the area, having a varied building enclosure and planting to alternate with verges, trees and front gardens. Focal buildings to be used along its length with incidental spaces to open out views.

- A Primary Rural Lane should be provided within development proposals which allow for a variation in building line but with a strong level of enclosure. Planting could alternate within verges and front gardens, which also vary in width. The Lane should be enclosed by a tighter urban grain. The surfacing includes a series of junction spaces, which serve to slow vehicle speeds. A number of key focal point buildings are located along its length, forming visual markers and defining the junction spaces. The Lane should be broken up with incidental spaces which provide views through to the Linear Park and define other connections through to the rural edge.
- An Arrival Green should be provided at the Gateway of the development; the existing hedgerow should be retained to define the edge of the space, but allowing open views into the site towards a Focal Point Green Square; there should be a continuous development frontage around the Gateway Green and Yew Tree Drive, required to minimise noise impacts to rear private gardens.
- Built form to be two storeys with potential for roof dormers, varied plots and building footprints to give visual interest and with some buildings clustered to give a village feel. Boundaries to be stone/rail or hedging with varied gaps between buildings to allow for landscape planting to have visual impact in the street scene.
- Landscape connections should be drawn in along the Village Lane around a focal point square, which is enclosed by a tighter urban grain. The square provides a visual and physical connection to a Rural Green and contains SUDS requirements.



FIGURE29:
ILLUSTRATIVE SKETCH OF THE WHINNEY

1. New boundary planting
2. Rural green
3. Green Corridor
4. Rural courtyards
5. Play
6. Square
7. Green link
8. The Lane

2.0 MASTERPLAN FRAMEWORK

2.5 CHARACTER AREA GUIDANCE

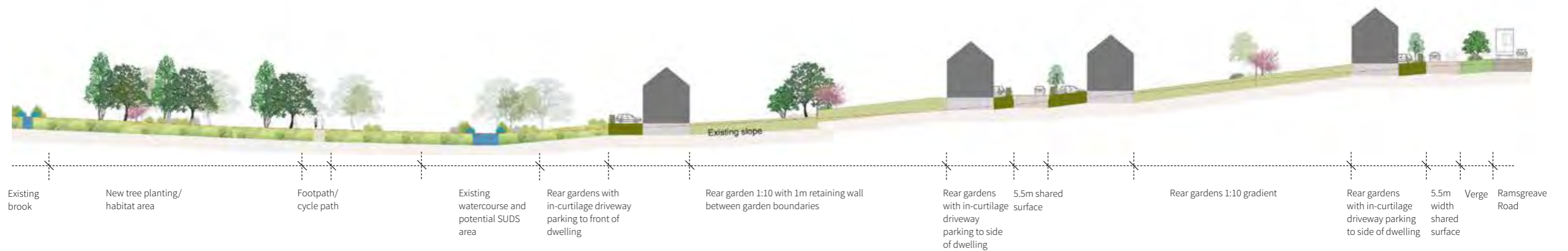


FIGURE 30
SECTION AA

- Within the other secondary/tertiary streets, any proposals for development should consider the provision of mews courtyards and greens or extend through to the Rural Cluster which sits beyond the Green Corridor; grain could be looser and more organic, opening up closer to the Linear Park, but tightening towards the existing built edge. A mix of properties should be provided. Buildings should either front or side onto the Green Corridor in order to provide a high degree of overlooking; varied roof forms (hips and gables) use of render, timber or stone within gables to create variety and interest when viewed from the Green Corridor. These streets should draw in the informal planting from the Green Corridor, which becomes more formal closer the existing built edge of Blackburn. Boundaries may include stone walls and hedges along the edge of the Green Corridor; with landscape planting flowing into the spaces and front gardens, which may be minimal within locations. All these edge characteristics will help to justify the proposals from a landscape and visual perspective.

These principles are illustrated within Figure 29 and the section in Figure 30.



ABOVE: CONTEMPORARY INTERPRETATION OF A DENSER HOUSING TYPOLOGY.



ABOVE: CONTEMPORARY INTERPRETATION OF A DENSER HOUSING TYPOLOGY, WHICH RETAINS SEMI-RURAL CHARACTER THROUGH THE USE OF STONE AND TIMBER.

2.0 MASTERPLAN FRAMEWORK

2.5 CHARACTER AREA GUIDANCE

2.5.3 THE WAVES

This area has a more urban character with a mix of brick and rendered properties, two stories in height of varying typologies which a character influenced by green streets and corridors. To be predominantly detached or semi-detached.

- The Waves should feature a mix of brick and rendered properties of varying typologies with a character influenced by green streets and corridors.
- Along the green corridor, buildings should either front or side on to the open space, providing a high degree of overlooking and maximising views out to the countryside. Variety and interest should be created with the use of varied roof forms (hips and gables) and changes in building materiality.
- The green link should bring the informal planting of the green corridor into the development, with planting becoming more formal as the development nears Ramsgreave Drive.
- Spaces which are courtyard in character should provide the interface with the green corridor, responding to landscape and visual sensitivities along the urban/rural transition.
- Rural Green - incorporates SUDS, seating/picnic areas and areas of informal landscape play.
- The visual impact of parked vehicles, particularly within the frontages of properties, should be kept to a minimum. re 31 and the sections in Figure 32 and 33.



FIGURE 31
ILLUSTRATIVE SKETCH OF THE WAVES



FIGURE 32
SECTION BB

1. New boundary planting
2. Rural green
3. Green Corridor
4. Public right of way
5. Rural courtyards
6. Play
7. Square
8. Green link
9. Village Lane

2.0 MASTERPLAN FRAMEWORK

2.5 CHARACTER AREA GUIDANCE



Right and far right: Traditional interpretation of Rural Typologies. Roofs step with changes in level.



Above: Local materials such as stone is used to create a rural character within smaller terraced housing, dwellings address a green space to create a 'village character'.



Above: Contemporary interpretation of larger family housing. Roof form creates variety within the street scene.

- The sections below identify a possible solution to for the steep levels within the site, whether that be through the provision of 1:10 rear gardens or by introducing retaining walls.
- It is expected that there will be a requirement to re-profile some of the land within the site to create workable gradients for access roads and gardens and this may reduce the overall requirement for utilising retaining walls within any development proposals. There needs to be flexibility and a variety of approaches to the topography within the site.

- Mitigation measures should be taken to reduce the impact of views of the site with a new green corridor. The green corridor should provide landscape screening and SUD's, as well as a transition between the development to the rural countryside. Buildings addressing the green corridor should be well set back to allow for this.

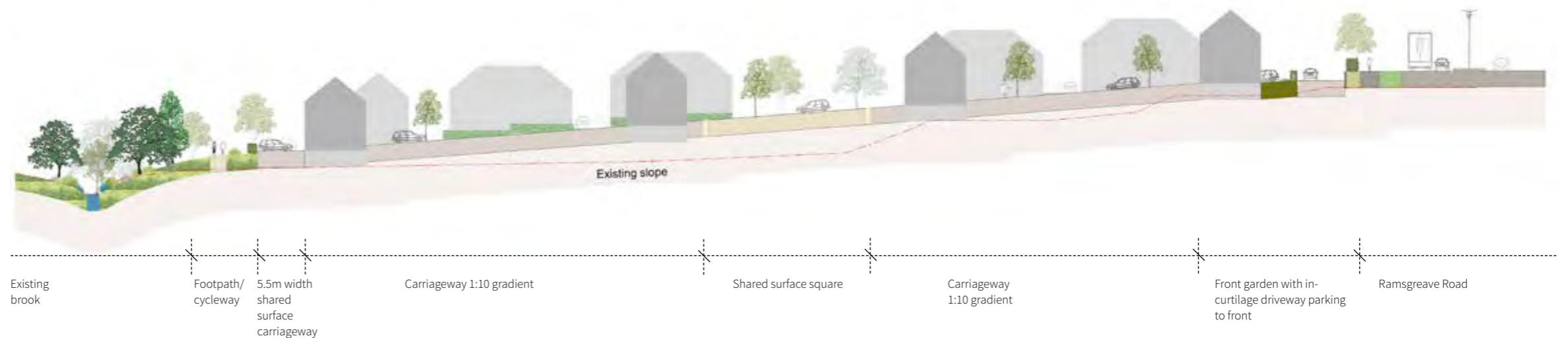


FIGURE 33
SECTION CC

2.0 MASTERPLAN FRAMEWORK

2.5 CHARACTER AREA GUIDANCE

2.5.4 BULLION MOSS FOLD

This part of the site will have a distinct rural characteristic, set between the countryside edge and the green corridor. It will take references from rural farmsteads and folds. Predominantly two storey buildings front onto courtyard spaces and lanes. It will be essentially organic in character.

- Bullion Moss Fold is organic in character with building forms defining the internal courtyard spaces. A mix of properties and buildings should either front or side onto the adjacent open space, providing a high degree of overlooking. Varied roof forms (hips and gables), combined with use of stone, brick, render and timber within gables and slate roofs, should create variety and interest when viewed from the green corridor.
- Boundaries could include stone walls and hedges and landscape planting should flow into the spaces and front gardens, which may be minimal within locations. Marker buildings define the gateway into Bullion Moss Fold.
- A substantial planting boundary should be provided along the interface with the open countryside, which will form a new boundary to the green belt.
- Key marker buildings should be used to define gateways into the character area.
- Building footprints and plots to be varied and buildings can be clustered to create enclosure to courtyard spaces.

These principles are illustrated within Figure 34.

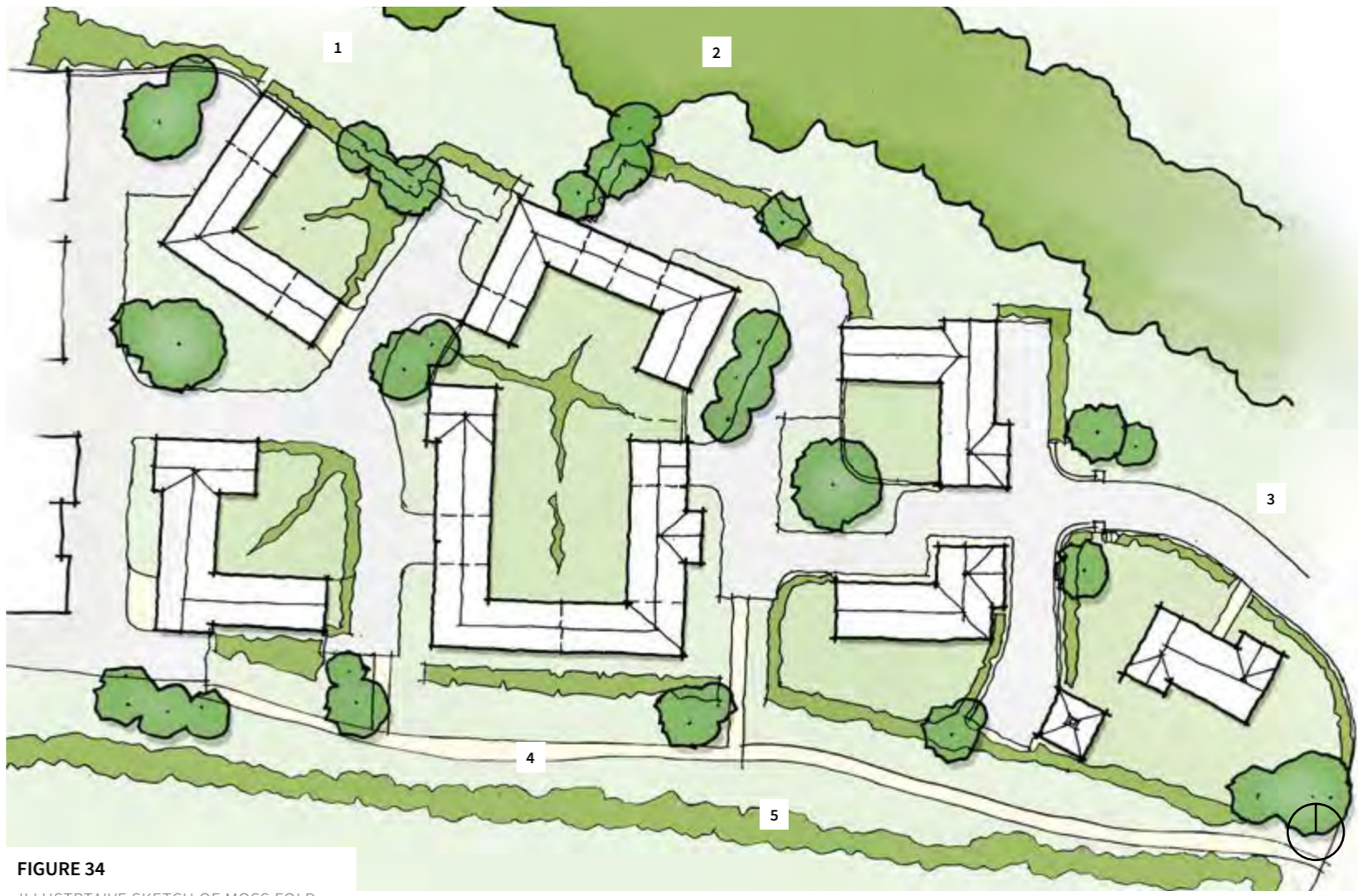


FIGURE 34
ILLUSTRATIVE SKETCH OF MOSS FOLD

1. Rural Courtyard
2. New boundary Planting
3. Rural Gateway
4. Green Corridor
5. Public Right of Way

Right and far right:
Contemporary
interpretation of Rural
Typologies.



2.0 MASTERPLAN FRAMEWORK

2.5 CHARACTER AREA GUIDANCE

2.5.5 WEST WILWORTH

This area will reflect the rural surroundings with an informal layout of clusters of dwellings. The green corridor will form a dominant feature within this area.

- Streets should be arranged to allow for development efficiency whilst orientating blocks to maximise views and connections to the open countryside to the north;
- Shared surfaces at junctions could create variety along the street and slow traffic;
- Variation in materials along the gables could help create visual markers;
- Spaces which are courtyard in character provide the interface with the Green Corridor and respond to landscape and visual sensitivities along the urban/rural transition.
- Consideration should be given to protecting the amenity of the existing properties fronting Ramsgreave Drive.
- Building footprints and plots sizes should be varied in size with variable gaps fronting the green corridor and allow for landscape planting to have visual impact . Hedges should be dominant boundary treatment, and building materials to be redbrick/ slate with render, stone buildings can front green corridor

These principles are illustrated within Figure 35.



FIGURE 35
ILLUSTRATIVE SKETCH OF WEST WILWORTH

1. New boundary planting
2. Green Corridor
3. Rural courtyards
4. Square
5. Primary Street
6. Privacy distances to existing dwellings are respected
7. Predominantly detached or semi-detached dwellings allow views from existing dwellings





3.0 Delivery

3.0 DELIVERY

3.1 INFRASTRUCTURE REQUIREMENTS AND PHASING

Infrastructure Requirements

A range of infrastructure will be required to support the new community and mitigate, if needed, any impacts of the development on the established community and/or existing infrastructure.

An Infrastructure and Delivery Plan (IDP) has been prepared to support delivery of the masterplan framework and is attached at Appendix 1.

The IDP sets out the infrastructure requirements/delivery organisation/the anticipated phase of development when this should be provided and the trigger point for provision.

The IDP has been based on:

- The Local Plan Part 2 Infrastructure and Delivery Plan (IDP), updated in January 2016¹. This provides a strategic level assessment of the infrastructure requirements needed to deliver the Local Plan Part 2 as a whole and has been produced in cooperation with information collected from stakeholders, key partners and service providers involved in delivering infrastructure.
- The Local Plan Part 2 housing land allocation Policy 16/2 – North Blackburn Development Site: Key Development Considerations draws on this information in setting out the key infrastructure requirements for developing out this site².
- Technical evidence prepared to inform the preparation of the masterplan. A full list of the

technical evidence can be provided on request from the Council. A copy of the Transport Assessment and Residential Travel Plan are available to view on the Council's website:

<http://www.blackburn.gov.uk/Pages/planning-land-property.aspx>

The key infrastructure requirements identified in the IDP are listed within the following themes; the infrastructure projects are listed in the IDP.

Themes:

- Access and transport
- Affordable housing
- Education
- Green infrastructure
- Surface water drainage
- Utilities

Taking the above into account, developers will be expected to provide a statement with their planning application to explain the contribution and the implication of individual scheme delivery on infrastructure requirements set out in the IDP.

The Council acknowledges that given the scale of development and multiple land ownerships there is a need to balance the certainty of delivery of key infrastructure with the need to maintain flexibility over the delivery of the development and infrastructure, especially as delivery is likely to take place over a number of years.

Additionally, the Council acknowledges that it may be necessary on a scheme by scheme basis to balance infrastructure requirements with the need to ensure that development remains viable. The developer's statement will establish the baseline for these considerations.

Phasing of Development

It is accepted that phasing of the development is dependent on landowners releasing land to the market and the masterplan accepts some flexibility with regards to delivery may be required. At the same time the Council will expect the delivery of infrastructure and phasing of the development to be approached in accordance with the following principles:

- Infrastructure should be provided in a timely way in order to reduce/mitigate the impact of the development;
- A coherent and coordinated approach to infrastructure delivery, construction management and development phasing is needed between land ownerships in order to ensure that the overall masterplan aspirations are met;
- Avoiding in so far as possible the creation of parcels of land or pockets of development that do not relate to their surroundings or are isolated from each other and from existing or proposed services and facilities;
- The early provision of transport and transport related infrastructure is recognised as being a key requirement in order to reduce the impact of the development upon the existing highway network, create a residential character alongside the dual carriageway and to establish connectivity with the surrounding area/community and local facilities;
- Regard is given to the different land ownerships on the site and the need to support development parcel release that facilitates early delivery of amenities whilst optimising revenues

to offset infrastructure investment.

The overall approach towards phasing and infrastructure delivery will need to ensure that each phase of development is as self-sufficient as possible whilst delivering necessary strategic elements of infrastructure and not prejudicing the ability of the following phases to do the same.

3.2 DEVELOPER CONTRIBUTIONS AND VIABILITY

Developer Contributions

Developer contributions will be required where off-site works are required to make a development acceptable or where a financial contribution is preferable to an on-site solution.

The masterplan's IDP sets out the full package of infrastructure requirements needed to make the overall development acceptable in planning terms; this includes infrastructure provided by the developer as an integral part of scheme delivery and/or, in some cases, delivered via a developer contribution.

As part of the masterplan preparations the viability of the masterplan proposals has been tested and an overall developer contributions envelope agreed. At development management stage the Council and the developer for the subject phase will agree a pro-rata contribution for that phase, to ensure that each part of the development appropriately contributes toward delivering the full package of infrastructure requirements set out in the IDP. The pro-rata contribution will be based on the number of dwellings within each proposed scheme/phase set against the total number of dwellings anticipated across the whole masterplan area. Within the limits of the developer contributions envelope there would be flexibility, if needed, to agree the bespoke package of infrastructure required at that stage of the overall masterplan site

1. The Infrastructure and Delivery Plan, January 2016 version, is available to view on the Council's website: www.blackburn.gov.uk/Pages/planning-land-property.aspx

2. The Infrastructure and Delivery Plan, January 2016 version, is available to view on the Council's website: www.blackburn.gov.uk/Pages/planning-land-property.aspx

3.0 DELIVERY



FIGURE 36
DIAGRAM ILLUSTRATING DEVELOPMENT PHASING



3.0 DELIVERY

development.

The Council considers this approach reflects the requirement for the overall development to deliver the infrastructure that makes the development acceptable as a whole, and it maintains equity between phases/ schemes coming forward at different times.

Access and Highways Improvements

A Transport Assessment (TA) and Residential Travel Plan have been prepared to support and inform the masterplan. The findings of these studies have identified the following transport/traffic measures that will need to be taken into account in scheme detailing and delivery:

- Vehicle access points on Yew Tree/ Ramsgreave Drive and one off Whinney Lane to facilitate the individually owned sites to be brought forward independently; the precise positions will be located as part of the preparation of individual scheme layouts;
- Enhanced pedestrian and cycle access/ crossings to improve accessibility and provide convenient and safe connectivity to the wider area, this will include a series of upgrades to existing the provision of new crossings;
- Improvements to the Yew tree Drive and Ramsgreave Drive corridor, and;
- A commitment to prepare, implement, deliver and manage a Travel Plan for the development, aiming to improve access to the development by all modes of transport through agreed measures and incentives thus reducing car dependency.

Although further work is required to establish detailed designs and costs, the TA demonstrates a firm commitment to deliver a safe and sustainable development. Further detail is contained within the 'Proposed Residential Development Yew Tree Drive and Ramsgreave Drive, Blackburn Transport Assessment (September 2016)'. The agreed works are set out in the Infrastructure and Delivery Plan in Appendix D. The Residential Travel Plan supports and complements the outcomes of the TA, and structures the Travel Plan based on the TA's measures put forward to address the traffic impact.

Education Requirements

The Council anticipates that the development of this site will trigger a requirement for additional capacity in local primary schools. The extent and timing of this requirement will be kept under review.

If through this review process and as part of an assessment at planning application stage, a specific requirement for additional primary school capacity is confirmed to exist, a proportionate developer contribution will be required towards the provision of additional capacity in local schools.

If sought, this developer contribution will remain within the overall developer contributions envelope identified as part of the viability testing for delivery of the masterplan.

There is sufficient capacity within the secondary schools within the borough to meet the increased demand linked to the planned housing growth set out in the Council's local plan.



3.0 DELIVERY

Affordable Housing

All new residential development is required to contribute towards meeting the identified need for affordable housing.

The Council would prefer, in this instance, to agree a commuted sum to provide affordable housing off site within the urban cores of the borough.

Section 106 Agreement

Blackburn with Darwen Borough Council does not have an adopted Community Infrastructure Levy Charging Schedule. Therefore, it is proposed that the impacts of the development will be mitigated, as needed and necessary infrastructure provided via a planning obligations agreement made under Section 106 of the Town and Country Planning Act 1990.

Developers are advised to enter into early discussions with the Council about the Section 106 agreement in order to determine appropriate draft heads of terms. Section 106 negotiations between the Council and developers will need to have regard to the Council's local plan, relevant supplementary planning documents and approved guidance including the requirements set out in this document. Full and up-to-date details can be discussed with the Development Management Team at planning application stage.

There will be a charge for the Council's legal fees for completing S106 Agreements. Details are available on the Council's website and can be discussed with Planning Officers at the development management stage.

Ensuring Viability

On the basis of the information considered and reviewed in the preparation of the masterplan the proposals set out in the masterplan framework are considered to be viable and deliverable.

The Council acknowledges that this position will be reviewed and tested as more comprehensive information is gathered and assessed alongside the preparation of detailed scheme proposals for individual land parcels for submission for planning consent.

The Council is committed to ensuring that an appropriate balance is struck between securing developer contributions for necessary public infrastructure investment and maintaining the financial viability of high quality development³.

In instances where a developer seeks to negotiate a reduced contribution towards infrastructure or other contribution that would normally apply due to financial viability, they will be expected to provide evidence to demonstrate the financial viability of the development. This should take the form of an open book financial appraisal of the proposed development using the Homes and Communities Agency's Development Appraisal Tool (DAT) model. The web link to the model:

www.gov.uk/government/collections/development-appraisal-tool

Further information on utilising the model including any required support to complete is available through the Homes and Communities Agency (HCA).



3. Local Plan Part 2: Policy 12 – Developer Contribution refers

3.0 DELIVERY

3.3 REQUIREMENTS FOR PLANNING APPLICATIONS

It has already been noted that the format of the masterplan is intended to provide some flexibility for individual scheme design and delivery. This approach reflects the likelihood that a number of separate planning applications by individual landowners/ developers/promoters will be submitted over time. At the same time it is essential to recognise the importance of securing consistency in approach, quality and co-ordination between applications and the delivery of infrastructure in accordance with the requirements of this document.

In order to avoid piecemeal development which may prejudice the overall delivery of a high quality and sustainable place the Council's preferred approach and information that is expected to be submitted with individual planning applications is as follows:

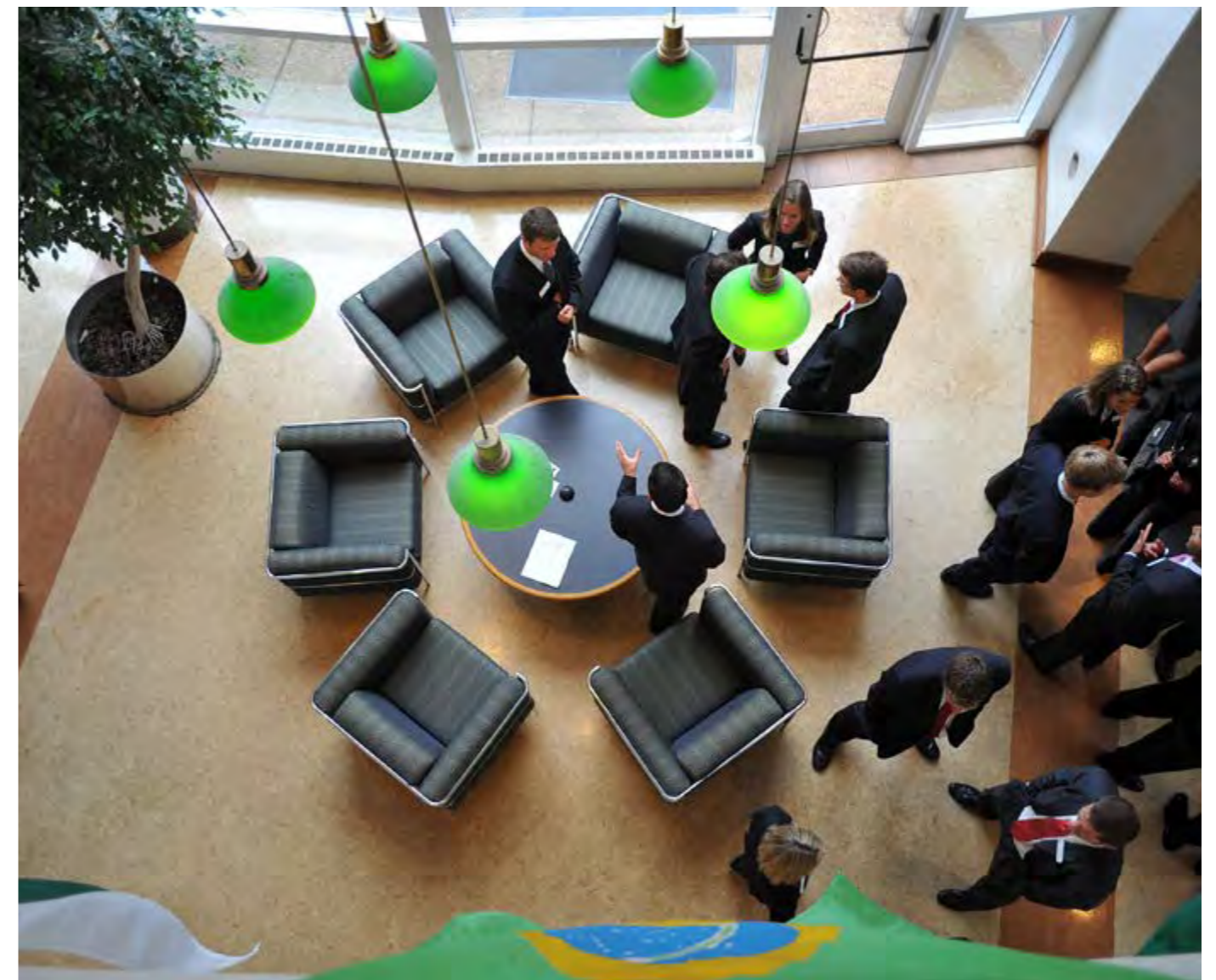
- Developers are encouraged to engage in pre-application discussions prior to submitting a planning application. Further advice on the Council's pre-application process and associated costs can be obtained via the following web address: <http://www.blackburn.gov.uk/Pages/Planning-advice.aspx>
- Planning applications must be supported in the usual way by appropriate documentation. Information regarding the Council's validation requirements and the procedure for applying for planning permission can be obtained on the Council website at: <http://www.blackburn.gov.uk/Pages/Planning-permission-applications.aspx>
- To enable the Council to make a full and proper assessment of the contribution and potential impact of each development proposal on the comprehensive development of the wider site, the Council will expect the information submitted

with each planning application to address the following:-

- Development framework. Applicants should use their Design and Access Statement and Planning Statements to demonstrate how they have incorporated high standards of design and to explain how the proposed development would fit together with, and help deliver, the wider masterplan.
- Infrastructure and Delivery Plan. Applicants will need to provide a robust explanation of how the proposed development is compliant with the overall infrastructure requirements of the masterplan area as set out in the IDP;
- Transport Assessment. Applicants will need to demonstrate that their proposals are based on a robust site wide assessment of the transport requirements, traffic impacts and associated mitigation measures, if needed, for the development, including the need to prepare, implement, deliver and manage a Residential Travel Plan.

Environmental Impact Assessment

Under the provisions of the Town and Country Planning (Environmental Impact Assessment) (Amendment) Regulations 2015 the development proposed may require an Environmental Impact Assessment. Consequently, to establish whether or not a planning application would require an EIA, a Screening Opinion may be sought by the applicant from the Planning Authority. 'Screening' is a procedure used to determine whether a proposed project is likely to have significant effects on the



3.0 DELIVERY

environment. It should normally take place at an early stage in the design of the project. However, it can also occur after a planning application has been made. If it is concluded that an EIA is required, a Scoping Opinion should then be sought to establish the extent of issues to be considered in the assessment and reported in an Environmental Statement. If an EIA is required, the Environmental Statement should be submitted along with the formal planning application taking into account the matters outlined in the Council's Scoping Opinion.

Community Engagement

Planning applications for development should demonstrate how the community have been engaged and consulted, and, how this has informed proposals. Community consultation should be undertaken in accordance with the principles set out in the Statement of Community Involvement.

Delivery and Monitoring

The success of the overall development will depend to a large extent on the continued collaboration of the landowners, the Council and other key stakeholders to secure the delivery of development including supporting infrastructure in a timely way.

The delivery of phases will be overseen by the Council working with landowners/developers/promoters and stakeholders to ensure:

- Delivery of the masterplan's housing requirements and associated facilities;
- A coordinated approach to infrastructure delivery in accordance with the agreed Infrastructure and Delivery Plan;
- The delivery of a consistently high quality of design of development in accordance with the guidance and principles set out in the masterplan and supporting documents;
- The delivery and management of the Residential Travel Plan ensuring the agreed measures and monitoring programmes are delivered in accordance with the approved Travel Plan Document and;
- An agreement is in place covering the delivery of the long term management and maintenance for the development.



4.0 Appendices

A SITE VISIBILITY AND KEY VIEWS

BASELINE VISUAL ASSESSMENT METHODOLOGY

Type Of View And Number Of Viewers

In terms of assessing the baseline visual sensitivity, key factors to consider are the type of view and the likely numbers of viewers (the visual receptors). The type of view and the number of viewers are described in the following terms:

- i) Glimpsed (i.e. in passing)/Filtered/Oblique/Framed/Open Views; and
- ii) Few/Moderate/Many Viewers

Value Of Views

The value attached to views has regard to a number of factors, including:

- recognition through planning designations or heritage assets; and
- the popularity of the viewpoint, its appearance in guidebooks, literature or art, on tourist maps and the facilities provided for its enjoyment.

The assessment of the value of views is summarised in Table 1 opposite, in terms of high, medium and low value. These criteria are provided for guidance only and are not intended to be absolute.

Susceptibility Of Visual Receptors To Change

The susceptibility of different types of visual receptor to changes in views is mainly a result of:

- The occupation or activity of the viewer at a given location; and
- The extent to which a person’s attention or interest may therefore be focussed on a view and the visual amenity experienced at a given view.

The assessment of a visual receptor to change is specific to the proposed development. However the Guidelines for Landscape and Visual Impact Assessment offers the generic guidance identified in Table 2 as a starting point for the assessment.

Overall Sensitivity Of Visual Receptors

The assessment of receptor sensitivity combines judgements on the susceptibility of the receptor to the specific type of development proposed and the value attributed to that receptor.

Value	Criteria
High	Views from landscapes/viewpoints of national importance, or highly popular visitor attractions where the view forms an important part of the experience, or with important cultural associations.
Medium	Views from landscapes/viewpoints of regional/district importance or moderately popular visitor attractions where the view forms part of the experience, or with local cultural associations.
Low	Views from landscapes/viewpoints with no designations, not particularly popular as a viewpoint and with minimal or no cultural associations.

Susceptibility	Type of Receptor
High	Residents; People engaged in outdoor recreation, including users of public rights of way, whose attention is likely to be focussed on the townscape and on particular views; Visitors to heritage assets or other attractions where views of the surroundings are an important part of the experience; Communities where views contribute to the townscape setting enjoyed by residents; and Travellers on scenic routes.
Medium	Travellers on road, rail or other transport routes, where the view is moderately important to the quality of the journey.
Low	People engaged in outdoor sport or recreation, which does not involve appreciation of views; People at their place of work, where the setting is not important to the quality of working life; and Travellers on road, rail or other transport routes, where the view is fleeting and incidental to the journey.

A SITE VISIBILITY AND KEY VIEWS

Location of View	Type of View & Number of Viewers	Value of Views	Susceptibility to Change	Overall Sensitivity of Receptor
View 1: View south-west from Whinney Lane	Glimpsed (passing) travelling views. Oblique view for residents. Moderate/few viewers.	Low	High/medium	Medium
View 2: View south-west from public footpath by Vine House Farm.	Wide views of eastern end of site, filtered by changes in landform and vegetation. Few viewers.	Low	High	Medium
View 3: View east from public footpath at Bullion Moss.	Wide views of eastern end of site (southern end). Views of northern end obscured by hedgerows. Few viewers.	Low	High	Medium
View 4a: Views east from Barker Lane/Lammack Road.	Glimpsed (passing) view for people travelling along Barker Lane. Moderate/few viewers.	Low	Medium	Medium/low
View 4b: Views west from Barker Lane/Lammack Road.	As above.	Low	Medium	Medium/low
View 5: View east from public footpath off Lammack Road.	Open views across the western end of the site (eastern land parcel). Moderate/few viewers.	Low	High	Medium
View 6: View north-east from Yew Tree Drive A6119.	Open views across the southern end of the western land parcel. High numbers of viewers.	Low	Low	Low
View 7: View north from public footpath off Ramsgate Road A6119.	Panoramic views across the eastern land parcel. Moderate numbers of viewers.	Low	High	Medium
View 8: View north from Blackburn Rugby Club off Ramsgate Road A6119.	Panoramic views across the eastern land parcel. High numbers of viewers when rugby club is in use.	Low	Low	Low
View 9: View north from public footpath off Ramsgate Road A6119 leading to Kay Fold Farm.	Panoramic views of the eastern end of the eastern land parcel. Few viewers.	Low	High	Medium
View 10: View west from public footpath off Pleckgate Road.	Open views of the eastern end of the eastern land parcel. Few viewers.	Low	High	Medium
View 11: View south/south-west from public footpath north off Broadhead Farm.	Elevated open views of the eastern end of the eastern land parcel. Rolling topography prevents views of the whole site. Few viewers.	Low	High	Medium
View 12: View south off Ramsgreave Road/Top of Ramsgreave.	Elevated open views of the allocated land. Moderate/few viewers travelling along Mellor Lane.	Low	Medium	Medium/low
View 13: View south from public footpath off Ramsgreave Road.	Open views across the western end of the site (eastern land parcel). Few viewers.	Low	High	Medium
Viewpoint 14: View south from Ramsgreave Road.	Elevated open views of the eastern land parcel. Moderate/few viewers travelling along Mellor Lane.	Low	Medium	Medium/low
Viewpoint 15: View west from public footpath at Brownhill.	Panoramic views of the townscape. Varied topography, hedgerows and tree cover prevent views of the whole site.	Low	High	Medium
Viewpoint 16: View west from public footpath at Upper Mickle Hey.	As above.	Low	High	Medium
Viewpoint 17: View north along Whinney Lane.	Glimpsed view for people travelling up Whinney Lane. Oblique views of western land parcel. Moderate/few viewers.	Low	Medium	Medium/low
Viewpoint 18: View north-east from A6119 Yew Tree Drive.	Fleeting travelling views of the western land parcel. High number of viewers.	Low	Low	Low
Viewpoint 19: View east from public footpath at Lower Reaps.	Open views towards the western land parcel. Tree cover, housing and changes in topography partially obscure site. Few viewers.	Low	High	Medium
Viewpoint 20: View east from public footpath at Dick Dadds.	Open views towards the western land parcel. Tree cover, housing and changes in topography partially obscure site. Few viewers.	Low	High	Medium
Viewpoint 21: View east from Nickey Lane, Mellor.	Glimpsed view from dwellings and for people travelling along Nickey Lane. Tree cover, housing and changes in topography partially obscure site. Moderate/low number of viewers	Low	High/medium	Medium
Viewpoint 22: View south-east from Mellor Lane, Mellor.	Glimpsed view for people travelling along Mellor Lane. Tree cover, housing and changes in topography partially obscure site. Moderate number of viewers.	Low	High/medium	Medium
Viewpoint 23: View south-east from Mellor Moor.	Elevated open view. Tree cover, housing and changes in topography partially obscure site. Moderate number of viewers. Few viewers	Low	High	Medium
Viewpoint 24: View east from public footpath south-east of Kingbank Farm.	Open views towards the western land parcel. Tree cover, housing and changes in topography partially obscure site. Few viewers.	Low	High	Medium
Viewpoint 25: View east from public footpath north-east of Kingbank Farm.	Open views towards the western land parcel. Tree cover, housing and changes in topography partially obscure site. Few viewers.	Low	High	Medium

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 1: VIEW SOUTH-WEST FROM WHINNEY LANE.



VIEWPOINT 2: VIEW SOUTH-WEST FROM PUBLIC FOOTPATH BY VINE HOUSE FARM.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 3: VIEW EAST FROM PUBLIC FOOTPATH AT BULLION MOSS.



VIEWPOINT 4A: VIEWS EAST FROM BARKER LANE/LAMMACK ROAD.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 4B: VIEWS WEST FROM BARKER LANE/LAMMACK ROAD.



VIEWPOINT 5: VIEW EAST FROM PUBLIC FOOTPATH OFF LAMMACK ROAD.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 6: VIEW NORTH-EAST FROM YEW TREE DRIVE A6119.



VIEWPOINT 7: VIEW NORTH FROM PUBLIC FOOTPATH OFF RAMSGATE ROAD A6119.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 8: VIEW NORTH FROM BLACKBURN RUGBY CLUB OFF RAMSGATE ROAD A6119.



VIEWPOINT 9: VIEW NORTH FROM PUBLIC FOOTPATH OFF RAMSGATE ROAD A6119 LEADING TO KAY FOLD FARM.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 10: VIEW WEST FROM PUBLIC FOOTPATH OFF PLECKGATE ROAD.



VIEWPOINT 11: VIEW SOUTH/SOUTH-WEST FROM PUBLIC FOOTPATH NORTH OFF BROADHEAD FARM.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 12: VIEW SOUTH OFF RAMSGREAVE ROAD/TOP OF RAMSGREAVE.



VIEWPOINT 13: VIEW SOUTH FROM PUBLIC FOOTPATH OFF RAMSGREAVE ROAD.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 14: VIEW SOUTH FROM RAMSGREAVE ROAD.



VIEWPOINT 15: VIEW WEST FROM PUBLIC FOOTPATH AT BROWNHILL.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 16: VIEW WEST FROM PUBLIC FOOTPATH AT UPPER MICKLE HEY.



VIEWPOINT 17: VIEW NORTH ALONG WHINNEY LANE.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 18: VIEW NORTH-EAST FROM A6119 YEW TREE DRIVE.



VIEWPOINT 19: VIEW EAST FROM PUBLIC FOOTPATH AT LOWER REAPS.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 20: VIEW EAST FROM PUBLIC FOOTPATH AT DICK DADDS.



VIEWPOINT 21: VIEW EAST FROM NICKEY LANE, MELLOR.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 22: VIEW SOUTH-EAST FROM MELLOR LANE, MELLOR.



VIEWPOINT 23: VIEW SOUTH-EAST FROM MELLOR MOOR.

A SITE VISIBILITY AND KEY VIEWS



VIEWPOINT 24: VIEW EAST FROM PUBLIC FOOTPATH SOUTH-EAST OF KINGBANK FARM.



VIEWPOINT 25: VIEW EAST FROM PUBLIC FOOTPATH NORTH-EAST OF KINGBANK FARM.

B TOWNSCAPE STUDY

● CORPORATION PARK



● BROWNHILL



● WILPSHIRE



● MELLOR



● MELLOR BROOK



● SHOWLEY FOLD

B TOWNSCAPE STUDY



A MIX OF A FORMAL AND INFORMAL LAYOUT. THE DEVELOPMENT ALLOWS CLEAR VIEWS OF THE SURROUNDING OPEN LANDSCAPE



THE BUILDING LINE CHANGES ALONG THE ROAD EDGE, PROVIDING A VARIETY OF BOUNDARY CONDITIONS



COURTYARD DEVELOPMENTS SIT BEHIND MAIN ROADS AND FORMAL TERRACES



A MIX OF GABLE ENDS AND FRONTAGES ADDRESS THE STREET AND ENSURE NATURAL SURVEILLANCE



B TOWNSCAPE STUDY

CORPORATION PARK CONSERVATION AREA

Scale and Urban Grain

The layout of the conservation area immediately adjacent to Corporation Park is typically made up of large semi-detached 3 storey villas, set within their own large plot. Further away from the park edge, but still within the conservation area, the urban grain becomes more closely knit, with medium sized terraces of 2 storeys.

Topography and Landscape

There is a steady incline within the Corporation Park area running from south to north. The top edge of the park sits on a plateau with views of Blackburn town centre to the south. For the most part the buildings run perpendicular to the contours, except for the properties which run along the ridge line, along Revidge Road.

The area is characterised by mature landscaping, hedges, large trees in gardens and on street, and grass verges. Views of the park are occasionally glimpsed in the northern parts of the area. Otherwise the park is only visible from the streets which follow it's boundary.

Gateways and Landmarks

There are no distinct gateways which mark the entrance into the conservation area. The area developed as part of its wider urban context, therefore there is no physical definition between it and the rest of Blackburn. However the distinctive grandeur and character of the housing marks it as unique within Blackburn. The Park is the landmark destination within Blackburn.

Boundaries and Parking

Most of the larger houses have large front gardens, with mature planting and hedges and stone walls clearly defining the edge of the plot. The parking is provided on plot on driveways. The smaller terraces mostly have small front gardens, with stone or brick wall boundaries and the parking is provided on street.

Materiality and Architectural Detailing

Within the conservation area, one of the characteristics which defines the area is the use of red brick and grey slate roofs. Stone features often, used for boundary walls, lintels, sills and some bay windows. The bay window is a feature of both the larger properties and the smaller terraces.



FIGURE 37: CORPORATION PARK

Plan to show the building, block and landscape layout of Corporation Park. The area is characterised by large urban blocks, with large detached properties within their own plots. The park sits in the heart of the area, with the gentle contours rising to the north of the area.

KEY

- Topography
- Public space
- Major route
- Secondary route
- Local route
- Surrounding existing buildings

B TOWNSCAPE STUDY



KEY CHARACTERISTICS

- Large Victorian housing in a mature landscape setting with smaller scale terraced streets surrounding the larger properties.
- Large urban blocks which mostly run perpendicular to the gradually increasing contours.
- Verdant quality to the streets, with street trees, verges, hedges and front gardens.
- Predominantly red brick and grey slate roofs, some stone detailing and bay windows a common architectural feature.

1. Red brick terrace properties run perpendicular to the contours. The roof line gentle slopes with the gradient, whilst the ground floors step up along the street. The bay windows set up a rhythm and animate the street elevation.
2. Some of the streets have mature street trees and grass verges which create a verdant quality to the streetscape.
3. Large three storey Victorian villas face the park, with large front gardens and stone walls defining the plot boundary and street edge.
4. Hedges and stone walls, with mature well established trees add to the greenery within the area.

B TOWNSCAPE STUDY

BROWNHILL AND WILPSHIRE

Scale and Urban Grain

Brownhill is made up of small 2 storey terraces which follow the main road leading out of Blackburn. Behind this linear development to the east the area has expanded and consists of a mix of larger terraced housing and suburban semi-detached post war properties.

Some of the older farmsteads sit close to the creeping urban edge, with very little open space between them.

Topography and Landscape

For the most part the main road and the railway line, and the development which follows it sit within a slight valley. There is a gradual incline northwards. Some of the housing in the east has been built on the surrounding hillsides and runs perpendicular to the topography.

Because of the nature of the development, the surrounding hills are barely visible from the main road, however the hillsides and fields reveal themselves within the rest of the area. The surrounding network of rural footpaths wind around and through the development, making the countryside easily accessible.

Gateways and Landmarks

There is no distinguishable gateway into the Brownhill area, apart from the road junction with the A6119. The building typologies and line of development follows that of development south of the ring road towards Blackburn town centre.

Boundaries and Parking

Most parking is provided on street, with some on plot for the larger semi-detached/ detached properties. There is no consistent boundary treatment, some properties sit back of pavement, whilst others have small brick walls and fencing.

Materiality and Architectural Detailing

The historic development following the main road features many traditional stone terraces, whereas the newer housing to the east mostly consists of red brick housing.

Key Characteristics

- The layout of Brownhill and Wilpshire are both informed by the line of the main roads leading out of Blackburn.
- The historic parts of the settlements follow the road and are made up of stone terraces. Infill development has occurred at the back of the original housing and consists of a mix of housing types and styles.
- The boundary between countryside and town has become blurred and the main road out of Blackburn. The development along the main road continues the urban characteristics of Blackburn, whereas the housing behind looks towards the rural edge.
- The materials are a mix of stone and different brick types and colours.
- The edges of the developments start to creep up, or down the surrounding hill sides.



FIGURE 38 - BROWNHILL

Plan to show the building, block and landscape layout of Brownhill. The area is characterised by ribbon development which follows the line of the main roads leading in and out of Blackburn. The hillsides surrounding the edges of the development.

KEY

- Topography
- Rural countryside
- Major route
- Secondary route
- Local route
- Built form

B TOWNSCAPE STUDY



1. The original stone cottages follow the line of the main road leading out of Blackburn
2. Post war semi-detached housing has been developed behind the main road moving up the surrounding hillsides
3. Behind the main road Brownhill is made up of a mix of terraces and semi-detached housing
4. The surrounding countryside and rural farmsteads sit close to the urban edge
5. The edge of the suburban housing has crept up the hillside into the rural areas

B TOWNSCAPE STUDY



1



2



3



4



5

1. The road structure follows the change in contours at the junction, key buildings with white rendered walls and increased heights emphasise the junction.
2. Further along the road, moving away from Blackburn, larger stone terraces directly face the street.
3. Closer to the countryside, the properties become detached and more dispersed and in some cases follow the line of the contours, and are terraced up the hillside.
4. The road structure follows the change in contours at key junctions, creating interesting corner buildings and built form.
5. Smaller scale terraced streets lead from the main road to the countryside beyond.

B TOWNSCAPE STUDY

4.5 MELLOR AND MELLORBROOK

Scale and Urban Grain

Mellor is an historic village which originally evolved following the line of Mellor Lane. Along this edge you find the oldest buildings mostly made up of short runs of stone terraces and individual stone cottages. The layout is a fine grain, closely knit, hugging the edge of the road. The majority of these buildings are no more than 2 storeys.

South of Mellor Lane the village has expanded. Several streets of post-war semi-detached houses and bungalows. Here the plots are slightly larger, with larger front and back gardens. The properties range from 1-2 storeys.

Topography and Landscape

Both the historic and newer buildings in Mellor respond to the topography in a variety of different ways. Most properties run perpendicular to the contours and either their roof line steps down the gradient in individual buildings, or small groups of 2 or 3. In other instances the roof line follows the gradient in a continuous line and the ground floors step with the change in level.

Some properties follow the contour line and buildings either side of a street sit at different levels, this has an impact on level access in some cases.

The edge of the village is clearly defined by the surrounding landscape. It is accessible from a network of footpaths, some of which run through the village.

Gateways and Landmarks

For the most part there is a clear boundary between the village and the surrounding countryside. As you approach the village along Mellor Lane from the east the first stone cottages mark the entrance. Travelling through the village the Church of St Mary is visible in the distance, marking the centre of the village. There are no other visible landmarks, however there are many views out from the village to landmarks within the surrounding countryside.

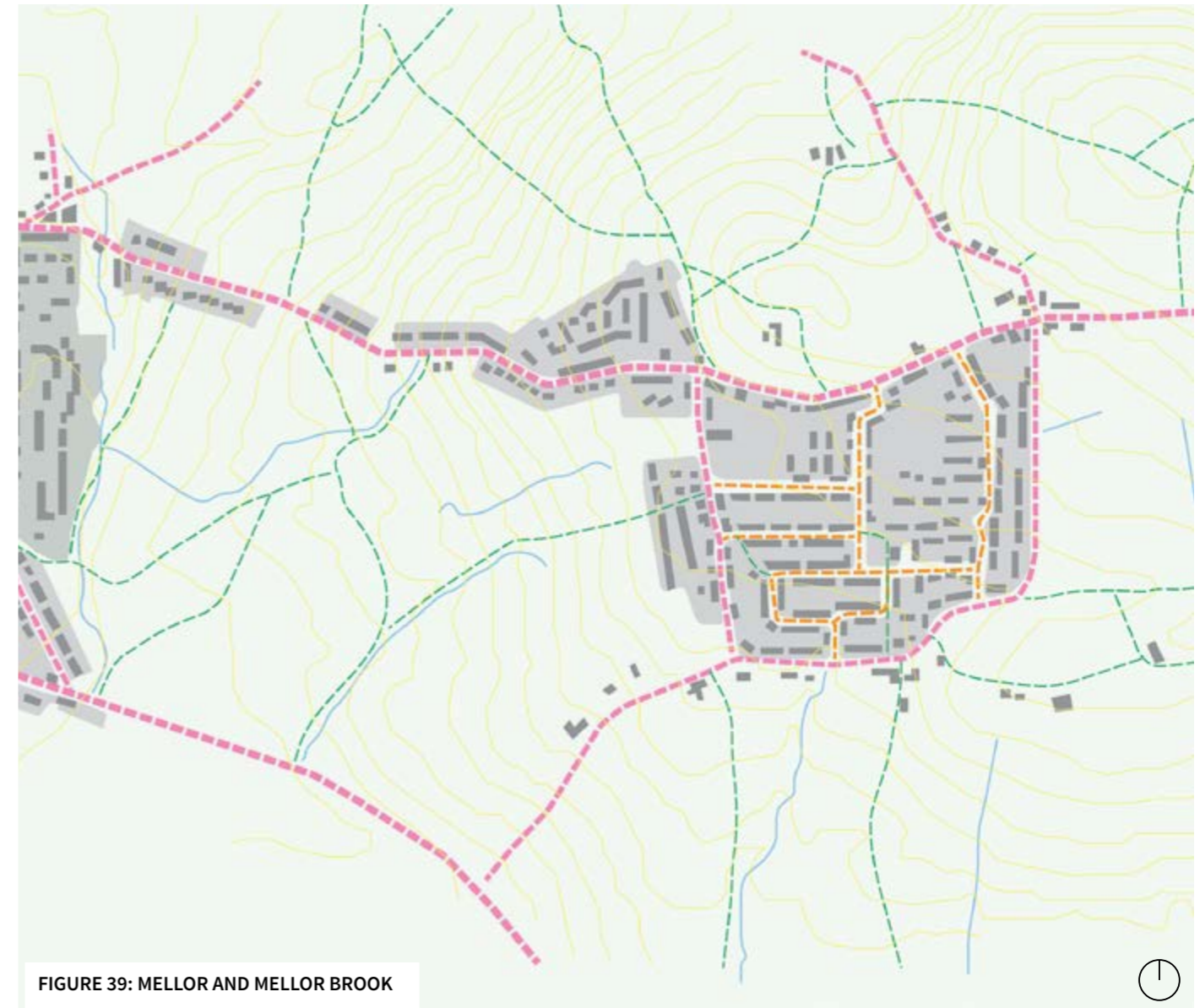


FIGURE 39: MELLOR AND MELLOR BROOK

Plan to show the building, block and landscape layout of Mellor and Mellor Brook. The original settlement follows the ridge line of Mellor ridge, with later development spreading out to the south

KEY

- Topography
- Public space
- Major route
- Secondary route
- Local route
- Built Form

B TOWNSCAPE STUDY



1. Approaching Mellor from the east there is a clear boundary between the rural and village edge
2. Stone buildings sit close to the road edge and step back in places to create courtyards or small parking areas
3. In parts the village directly faces the wider countryside with long range views
4. The village has expanded to the south. A small development of post war housing follows the contours and steps down the hillside
5. As you enter the village the spire of the village church acts as a landmark, marking the centre.
6. Traditional properties have small porches which line the road edge.

B TOWNSCAPE STUDY

Boundaries and Parking

Most parking is provided on street, with a few on plot for the larger semi-detached properties and single cottages.

The predominant boundary feature is the stone wall. This feature continues from the field boundaries into the heart of the village. Hedges also appear, combined with stone walls and within the post war housing.

Materiality and Architectural Detailing

The historic development following the main road features many traditional stone terraces, whereas the newer housing to the south of Mellor Lane is a mix of red brick and white render.

The traditional cottages have a number of defining characteristics. Large stone lintels and sills surrounding the windows. Small porches are also a feature of many properties, protecting the entrances from strong winds.



B TOWNSCAPE STUDY



Key Characteristics

- Traditional housing follows the ridge line of Mellor Lane and the hillside as it descends into Mellor Brook
- There is a clear boundary between the settlements and the surrounding countryside
- The traditional housing has some distinct characteristics, such as stone lintels and sills, porches, very close proximity to the roadside
- Open views of the countryside are frequently present from within the developments, either terminating views along a road or properties fronting directing on to the open countryside.
- Properties step with the contours either at roof level, or in some case at the ground floor. This creates a variety of typological solutions.

1. Some of the older properties sit below the road edge, with their entrances much lower than the properties opposite
2. Other terraces, the roof line follows the gradient, and the ground floor steps with the change in level
3. Some buildings, although close to the road, have very small front gardens, with mature planting which encloses the road and limits the view
4. Occasionally the change in contours create dramatic relationships between adjoining buildings, the road and the view ahead
5. A change in road alignment, opens up opportunities for views of the wider countryside
6. Occasionally the change in contours create dramatic relationships between adjoining buildings, the road and the view ahead

B TOWNSCAPE STUDY

SHOWLEY FOLD

Scale and Urban Grain

The countryside north of Blackburn is scattered with small farmsteads and tiny hamlets. An example of one is Showley Fold which is situated along the Showley Road.

The layout consists of a cluster of 3 or 4 buildings. Historically these would have been the main farm house and several barns or outhouses. These buildings range from 3 to 1 storeys. Today some of these farmsteads have been converted to residential dwellings.

Topography and Landscape

The farmsteads tend to nestle within the landscape around them, alongside brook edges, close to hillsides and amongst small copses of trees.

Gateways and Landmarks

Often the entrance to the farmstead is marked by a building which addresses the road on which it is situated, forming a clear corner into the courtyard around which the buildings are arranged.

Boundaries and Parking

Parking is provided within the courtyard around which the buildings sit.

The boundaries of the farmsteads are defined by a mix of stone walls, the gable of stone buildings or landscape features such as trees, hedges or water courses.

Materiality and Architectural Detailing

Most of the examples within the study area are built of traditional local stone with slate roofs. Like the local terraces, the openings of the buildings are defined by large stone lintels and sills.

A feature of the roofs of the lower buildings is that they are steeply pitched with the eaves coming down within reaching height.

Key Characteristics

- Small clusters of stone buildings, arranged around a shared courtyard.
- The buildings comfortably sit within the rural landscape, surrounded by landscape features, footpaths, small brooks, trees, and hedges.
- Corner building defines the entrance to the small development and is usually visible from the country lane
- Interesting roofscapes of large sloping slate roofs.



FIGURE 40: SHOWLEY FOLD

Diagram to show the building and landscape layout of Showley Fold. The small cluster of buildings nestle in the surrounding countryside close to the edge of the country lane. Landscape features surround and define the boundaries of the hamlet.

KEY

- Topography
- Public space
- Major route
- Secondary route
- Local route
- Built Form

C POLICY 16/2

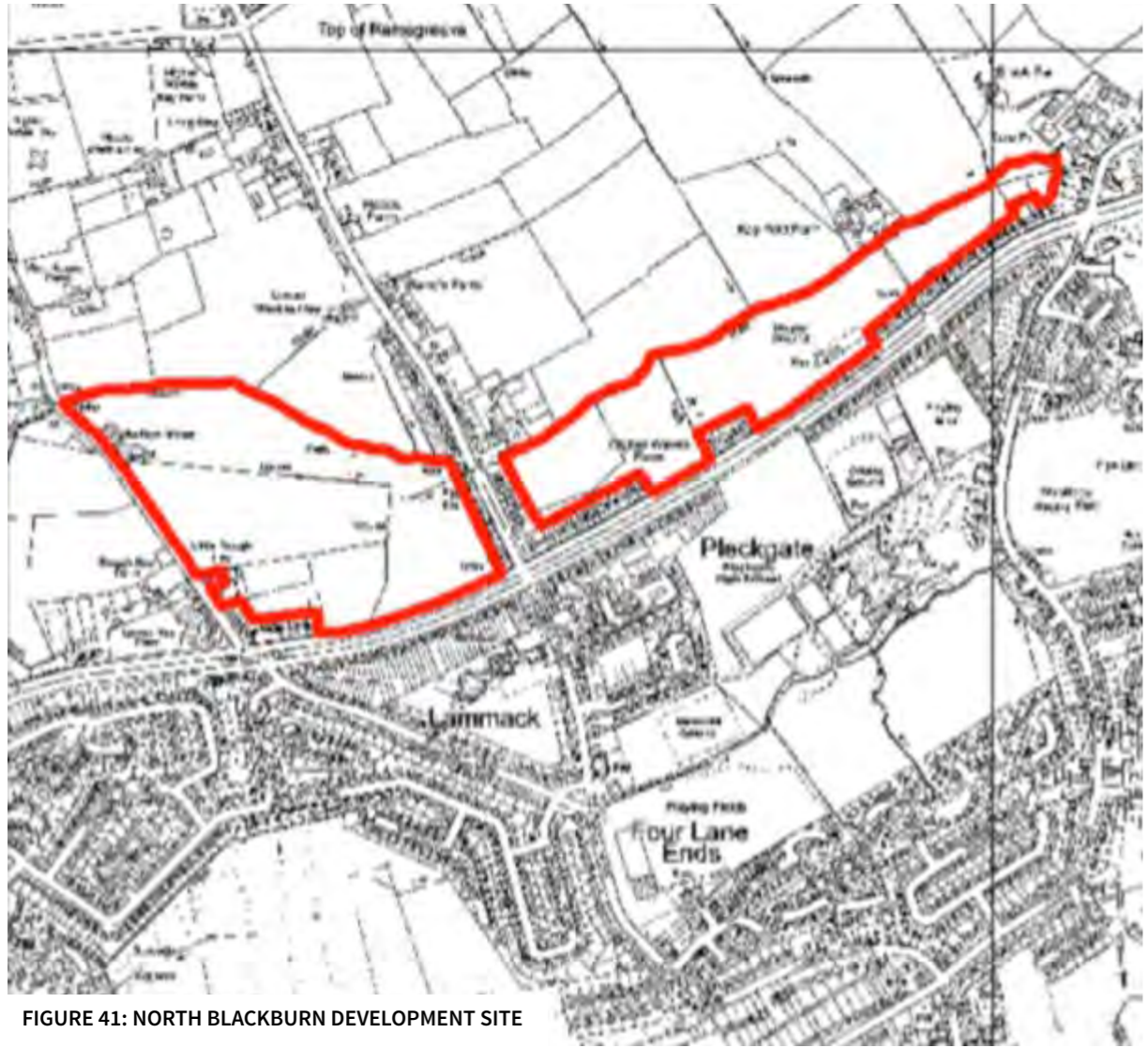


FIGURE 41: NORTH BLACKBURN DEVELOPMENT SITE

Site Area	28.8 hectares
Site Location	Edge of urban area
Housing Delivery	The estimated number of houses for this site is 450, of which 140 are expected to be delivered by March 2019

The land parcels occupy the space between the built up area of Blackburn and the brook which forms the boundary with the district of Ribbles Valley. The land to the east of Barker Lane / Lammack Road comprises the existing Rugby Club. The land to the west of Barker Lane / Lammack Road comprises improved grassland which is surrounded by trees and hedges.

KEY DEVELOPMENT CONSIDERATIONS

1. This site is to be brought forward in line with a masterplan to be produced covering the whole of the allocation. The masterplan must be agreed by the Council prior to the granting of planning permission for any part of the site.
2. The development should complement the existing residential areas as widening the existing range and choice of housing to meet local needs.
3. Development design – need for attractive scheme that is sympathetic to local area and character. The provision of a rural transition zone between the development and the wider rural landscape to the north, including the establishment of a robust boundary for the Green Belt through landscaping and planting.
4. The site currently occupied by Blackburn Rugby Union Football Club cannot be developed unless and until the Club has relocated within Blackburn with Darwen.
5. Allow for the retention and enhancement of the public right of way that runs around the edge of the site.
6. Be supported by a landscape and green infrastructure framework incorporating perimeter woodland planting and on site open space incorporating formal and informal play. Utilising important key vistas into the adjoining open countryside, providing visual linkages to Mellor Ridge.
7. Be supported by detailed survey to consider the impact of proposed development on the ecological value of the site, including the brook to the northern boundary.
8. Provision of SuDS and the incorporation of measures to control surface water run-off, flood risk from the northern watercourses and the consequences of blockages in the culvert.
9. The number of access points on to the A6119 should be informed by a detailed traffic impact assessment to ensure the free flow of traffic and highway safety.
10. Make land available for a new primary school, if required, and suitable contribution towards its construction.
11. Contribution towards local highways improvements.
12. The separation distance between the site and the road is minimal and therefore the noise generated by road traffic may potentially be high.
13. Completion of appropriate ground investigation works to establish the extent of any ground contamination and whether any mitigation measures are required.

D Infrastructure and Delivery Plan (IDP)

ACCESS AND TRANSPORT (off-site highway works)

INFRASTRUCTURE/THEME TYPE	INFRASTRUCTURE REQUIREMENT/S	DELIVERY ORGANISATION/S; DELIVERY MECHANISM	ANTICIPATED PROGRAMMING OF WORKS	COMMENTS
Junction improvements				
To improve pedestrian and cyclist accessibility/connectivity and safety				
Brownhill roundabout/junction cycle linkage	Improvements for cycle and pedestrian linkage/connectivity	BwD - 100% development funded via S106 funding	Phase B	A preliminary scheme has been prepared. Refer to Transport Assessment (September 2016) Section 8: Potential developer contributions to infrastructure and accessibility improvements
Ramsgreave Drive/ Pleckgate Road junction	Upgraded crossing facility to incorporate Toucan crossing	BwD - 100% development funded via S106 funding	Phase B	
Yew Tree Drive/ Lammack Road junction	Provision of Toucan crossing	BwD - 100% development funded via S106 funding	Phase A	
Crossing close to/between Whinney Lane/Lammack Road	Provision of Toucan crossing	BwD - 100% development funded via S106 funding	Phase A	This crossing may be provided as a stand-alone feature or integrated within the new site access/junction into Phase A off Yew Tree Drive
A6119 Yew Tree Drive/Ramsgreave Drive Corridor Improvements:				
To create a residential character/environment along this length of the dual carriageway including features and measures to address severance and manage traffic speeds.				
Whinney Lane to Pleckgate Road	Work/schemes to support speed limit reduction and establish a sense of place including but not limited to: - Promotion of Traffic Regulation Orders	BwD- 100% development funded via Sec 106 funding	Agreed costs pro- rata between Phases A and B based on baseline viability appraisal for the masterplan	An agreed package of works to complement the new junctions and crossing points at the new site access points along the corridor.

D Infrastructure and Delivery Plan (IDP)

ACCESS AND TRANSPORT (off-site highway works)

INFRASTRUCTURE/THEME TYPE	INFRASTRUCTURE REQUIREMENT/S	DELIVERY ORGANISATION/S; DELIVERY MECHANISM	ANTICIPATED PROGRAMMING OF WORKS	COMMENTS
	<ul style="list-style-type: none"> - Removal of central crash barrier between Whinney Lane and Pleckgate Road; - Gateway features, and; Landscape works including treatment of the central reservation. 			
<p>Junctions forming new site access/es off Yew Tree Drive/Ramsgreave Drive (A6119):</p> <p>To open up the individual land parcels for development</p>				<p>TA has set out a number of access points along the A6119 to enable land parcels in separate ownerships to be developed independently.</p> <p>Details of the configuration of each junction are to be agreed at development management stage and subject to S278 agreement/s.</p>
<p>Yew Tree Drive access serving western parcel</p>		<p>Developer; S278 scheme/s</p>	<p>Phase A</p>	
<p>Whinney Lane access serving western parcel</p>		<p>Developer; S278 scheme/s</p>	<p>Phase A</p>	
<p>Ramsgreave Drive access serving eastern land parcel to west of Higher Waves Farm (eastern site access 1)</p>		<p>Developer; S278 scheme/s</p>	<p>Phase B</p>	
<p>Ramsgreave Drive access serving eastern site close to rugby club (eastern site access 2)</p>		<p>Developer; S278 scheme/s</p>	<p>Phase B</p>	

D Infrastructure and Delivery Plan (IDP)

TRAVEL PLAN

INFRASTRUCTURE/THEME TYPE	INFRASTRUCTURE REQUIREMENT/S	DELIVERY ORGANISATION/S; DELIVERY MECHANISM	ANTICIPATED PROGRAMMING OF WORKS	COMMENTS
Travel Plan		<p>100% Developer contributions via ring-fenced Sec 106 funding.</p> <p>Agreed measures to be costed, costs to be approved by BwD, to be delivered by developer from the Sec 106 contributions.</p> <p>Management company once site is fully occupied.</p>	All	<p>A range of Travel Plan measures have been agreed in principle; these are detailed in the <i>Residential Travel Plan Framework (September 2016)</i> and accompanying appendices/documents and include:</p> <ul style="list-style-type: none"> - Community Rail Lancashire Residential Packs - Site specific Residential Travel Pack - Cycle storage arrangements (BykeBins and Sheffield Stands) - Personalised journey planning - Provision of residents' car club (via Co-Wheels including provision of 2 cars/free membership and introductory driving credit) - Provision of a free months adult bus ticket - Signage within the site for Weavers Wheel Cycle Scheme <p>Developer to appoint a Travel Plan Co-ordinator to work with BwD to agree delivery and costs of measures to be released on completion. The Travel Plan sets out the plan monitoring and assessment requirements, including an action plan and potential targets. Annual progress reports will be required by the Council.</p>

D Infrastructure and Delivery Plan (IDP)

AFFORDABLE HOUSING

INFRASTRUCTURE/THEME TYPE	INFRASTRUCTURE REQUIREMENT/S	DELIVERY ORGANISATION/S; DELIVERY MECHANISM	ANTICIPATED PROGRAMMING OF WORKS	COMMENTS
	Affordable housing required within a range of up to 20% subject to scheme viability.	BwD via S106 funding	All	<p>Requirement to meet Core Strategy Policy CS8: Affordable housing requirements:</p> <p><i>'All new residential development will be required to contribute towards meeting the identified need for affordable housing'</i></p> <p>The Council's preference is for the affordable housing to be provided off-site. The Council will seek to agree a commuted sum in accordance with approved Council guidance on affordable housing at development management stage.</p> <p>The Council will expect scheme viability assessment to use the Homes and Communities Agency's Development Appraisal Tool (DAT) model.</p>

EDUCATION

INFRASTRUCTURE/THEME TYPE	INFRASTRUCTURE REQUIREMENT/S	DELIVERY ORGANISATION/S; DELIVERY MECHANISM	ANTICIPATED PROGRAMMING OF WORKS	COMMENTS
	Additional primary school places.	BwD via S106 funding	All	
Brownhill roundabout/junction		<p>100% Developer contributions via ring-fenced Sec 106 funding. Agreed measures to be costed, costs to be approved by BwD, to be delivered by developer from the Sec 106 contributions.</p> <p>Management company once site is fully occupied.</p>		<p>Framework (September 2016) and accompanying appendices/documents and include:</p> <ul style="list-style-type: none"> -Community Rail Lancashire Residential Packs -Site specific Residential Travel Pack -Cycle storage arrangements (BykeBins and Sheffield Stands) -Personalised journey planning -Provision of residents' car club (via Co-Wheels including provision of 2 cars/free membership and introductory driving credit) -Provision of a free months adult bus ticket -Signage within the site for Weavers Wheel Cycle Scheme <p>Developer to appoint a Travel Plan Co-ordinator to work with BwD to agree delivery and costs of measures to be released on completion. The Travel Plan sets out the plan monitoring and assessment requirements, including an action plan and potential targets. Annual progress reports will be required by the Council.</p>

D Infrastructure and Delivery Plan (IDP)

GREEN INFRASTRUCTURE				
INFRASTRUCTURE/THEME TYPE	INFRASTRUCTURE REQUIREMENT/S	DELIVERY ORGANISATION/S; DELIVERY MECHANISM	ANTICIPATED PROGRAMMING OF WORKS	COMMENTS
	Green corridor along northern boundary of housing allocation site including integrated facilities such as footway and cycle network and informal play areas.	Developers	All	Delivered as phases are built out. Ongoing maintenance and management to be provided by developers via a maintenance company financed through a householder service charge or alternative agreed model.
	Provision of public green spaces within the housing blocks.	Developers	All	The Council will require a comprehensive and consistent approach across the whole housing allocation site.
	Green Gateways	Developers	All	The Council recognises that the 2 principal land parcels are likely to have separate management companies. The expectation is that the approach/standard of maintenance and management will be the same across the housing allocation site.
	Roadside including street planting/landscape works	Developers	All	Delivered as phases are built out. Need to discuss what would be maintained as part of the adopted highway and via private developer arrangement.

D Infrastructure and Delivery Plan (IDP)

SURFACE WATER DRAINAGE				
INFRASTRUCTURE/THEME TYPE	INFRASTRUCTURE REQUIREMENT/S	DELIVERY ORGANISATION/S; DELIVERY MECHANISM	ANTICIPATED PROGRAMMING OF WORKS	COMMENTS
	<p>Details of strategy and techniques for providing Sustainable Urban Drainage System (SuDs) – roadside and within green corridor – are to be agreed between developers, United Utilities, the Environment Agency and Blackburn with Darwen Council as Lead Local Flood Authority.</p>	<p>Developers</p>	<p>All</p>	<p>Overall strategy for surface water management will be required. To be discussed at planning application stage.</p> <p>SuDs features will remain in the developers’ ownership. Ongoing maintenance and management to be provided by developers via a maintenance company financed through a householder service charge or alternative agreed model. The Council will require a comprehensive and consistent approach across the whole housing allocation site.</p> <p>The Council recognises that the 2 principal land parcels are likely to have separate management companies. The expectation is that the approach/standard of maintenance and management will be the same across the housing allocation site.</p>

D Infrastructure and Delivery Plan (IDP)

UTILITIES				
INFRASTRUCTURE/THEME TYPE	INFRASTRUCTURE REQUIREMENT/S	DELIVERY ORGANISATION/S; DELIVERY MECHANISM	ANTICIPATED PROGRAMMING OF WORKS	COMMENTS
Gas	Connection to existing infrastructure	Developers/National Grid		
Electricity	Connection to existing infrastructure	Developers/National Grid/Electricity North West.		
Water	Provision of connections and possible upgrades to local water supply where required	Developers/United Utilities		
Wastewater	Provision of connections to existing public or combined sewer and possible reinforcements to existing infrastructure where required	Developers/United Utilities		
Telecommunications/digital connectivity	Provision of connections	Developers/service providers		
	Roadside including street planting/landscape works	Developers	All	Delivered as phases are built out. Need to discuss what would be maintained as part of the adopted highway and via private developer arrangement.



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